



Spagnuolo & Company Real Estate Lawyers



Explanation of Covenant Registration Number CA9164173 to CA9164175

This charge is a Covenant. A Covenant generally imposes either a negative or positive condition on the property and often contains restrictions on building or other uses in favour of a municipality or the province.

This is a non-financial charge, meaning there is no money owing by the owner of the land to the charge holder. Also, this charge “runs with the land”, so it will remain on title following the sale to a new purchaser/buyer. This means that the obligations under this charge will be binding on all future owners.

This is a “development covenant” in favour of the City of Maple Ridge. The lands are part of a master planned mixed-use residential & commercial development. This charge requires the owner/developer to comply with attached design guidelines for the property. These guidelines show the location of buildings, road & sidewalk infrastructure, and landscaping; and provide specific guidance for construction of the neighbourhood and buildings. Further, the covenant requires the developer to include charging stations within parking facilities.

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Land Title Act

Charge

General Instrument – Part 1

NEW WESTMINSTER LAND TITLE OFFICE

JUL 06 2021 10:43:47.002

CA9164173-CA9164175

1. Application

KOFFMAN KALEF LLP
19TH F, 885 W. GEORGIA ST
VANCOUVER BC V6C 3H4
604-891-3619

2. Description of Land

| PID/Plan Number | Legal Description |
|--------------------|--|
| 030-857-210 | LOT 1 DISTRICT LOT 399 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP88082 |

3. Nature of Interest

| Type | Number | Additional Information |
|---------------------------|--------|---|
| COVENANT | | |
| PRIORITY AGREEMENT | | Granting the above Covenant priority over Mortgage CA8399561 and Assignment of Rents CA8399562 |
| PRIORITY AGREEMENT | | Granting the above Covenant priority over Mortgage CA8510513 and Assignment of Rents CA8510514 |

4. Terms

Part 2 of this instrument consists of:

(b) Express Charge Terms Annexed as Part 2

5. Transferor(s)

487559 B.C. LTD., NO.BC487559**BANK OF MONTREAL (AS TO PRIORITY)****BRITISH COLUMBIA HOUSING MANAGEMENT COMMISSION (AS TO PRIORITY)**

6. Transferee(s)

CITY OF MAPLE RIDGE
11995 HANEY PLACE
MAPLE RIDGE BC V2X 6A9

7. Additional or Modified Terms



Land Title Act

Charge

General Instrument – Part 1

8. Execution(s)

This instrument creates, assigns, modifies, enlarges, discharges or governs the priority of the interest(s) described in Item 3 and the Transferor(s) and every other signatory agree to be bound by this instrument, and acknowledge(s) receipt of a true copy of the filed standard charge terms, if any.

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

PATRICK J. JULIAN
Barrister & Solicitor
19TH FLOOR - 885 WEST GEORGIA
STREET
VANCOUVER BC V6C 3H4

YYYY-MM-DD

2021-06-18**487559 B.C. LTD.**

By their Authorized Signatory

TOM MEIER**Officer Certification**

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

CATHERINE A. SCHMIDT
Commissioner for Taking Affidavits
for British Columbia
11995 Haney Place
MAPLE RIDGE BC V2X 6A9

YYYY-MM-DD

2021-07-05**CITY OF MAPLE RIDGE**

By their Authorized Signatory

MICHAEL MORDEN, MAYOR

Expires: January 31, 2022

(as to both signatures)

STEPHANIE NICHOLS, CORPORATE
OFFICER

Officer Certification

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.



Land Title Act

Charge

General Instrument – Part 1

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

KENNETH WEI JAE BONG
Commissioner for Taking Affidavits
for British Columbia
 6TH FLOOR - 595 BURNARD STREET
 VANCOUVER BC V7X 1L5

YYYY-MM-DD

2021-06-21**BANK OF MONTREAL**

By their Authorized Signatory

GREG VRIEND, Managing Director,
Real Estate Finance

Officer Certification

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

Witnessing Officer Signature

Execution Date

Transferor Signature(s)

CHARLOTTE K. WONG
Barrister & Solicitor
 2110 Burquitlam Drive
 Vancouver BC V5P 2P1

YYYY-MM-DD

2021-06-24**BRITISH COLUMBIA HOUSING
MANAGEMENT COMMISSION**

By their Authorized Signatory

Armin Amrolia

Officer Certification

Your signature constitutes a representation that you are a solicitor, notary public or other person authorized by the *Evidence Act*, R.S.B.C. 1996, c.124, to take affidavits for use in British Columbia and certifies the matters set out in Part 5 of the *Land Title Act* as they pertain to the execution of this instrument.

Electronic Signature

Your electronic signature is a representation that you are a designate authorized to certify this document under section 168.4 of the *Land Title Act*, RSBC 1996 c.250, that you certify this document under section 168.41(4) of the act, and that an execution copy, or a true copy of that execution copy, is in your possession.

Daniel Solomon
Remick 1NQLH

Digitally signed by
 Daniel Solomon Remick
 1NQLH
 Date: 2021-07-05
 17:16:46 -07:00

TERMS OF INSTRUMENT – PART 2**SECTION 219 COVENANT – NO BUILD (PHASE 1)****2017-061-RZ**

THIS AGREEMENT dated for reference the _____ day of June, 2021 is

BETWEEN:

487559 B.C. LTD.
22367 Dewdney Trunk Road
Maple Ridge, British Columbia, V2J 3J4

(the “Covenantor”)

AND:

CITY OF MAPLE RIDGE
11995 Haney Place
Maple Ridge, British Columbia, V2X 6A9

(the “City”)

WHEREAS:

A. The Covenantor is the registered owner in fee simple of certain lands in the City of Maple Ridge, British Columbia which are legally described as:

1. Parcel Identifier: 030-857-210, Lot 1, District Lot 399, Group 1, New Westminster District, Plan EPP88062;

(the “Lands”).

B. The Covenantor wishes to construct a certain number of residential dwelling units, townhouses, street oriented units and some mixed use commercial and rental residential apartment buildings on the Lands and adjoining lands in seven phases as generally shown on the sketch plan attached hereto as Schedule “A”, and in accordance with the development concept generally shown in the concept plan attached hereto as Schedule “B”, and for that purpose the Covenantor has applied to the City to rezone the Lands as set out in Maple Ridge Zone Amending Bylaw No. 7336-2017 (the “Zone Amending Bylaw”).

C. The Covenantor has agreed that the Lands shall be developed in accordance with the requirements of this Agreement.

D. Section 219 of the *Land Title Act* provides, inter alia, that a covenant, whether of a negative or positive nature, may be registered as a charge against the title to the land, in favour of the City and that the covenant is enforceable against the Covenantor and the successors in title of the Covenantor.

E. A covenant under Section 219 of the *Land Title Act* may include provisions in respect of the use of land, the use of a building on or to be erected on lands; that land is to be built on

in accordance with the covenant, is not to be built on or that certain buildings are not to be used except in accordance with the provisions of the covenant or; that land is not to be subdivided unless in accordance with the covenant or is not to be subdivided at all.

- F. The Covenantor agrees to the restrictions in the uses of the Lands, the density of building on the Lands and the use and occupancy of all improvements on the Lands on the terms and conditions herein provided for.

NOW THEREFORE THIS AGREEMENT WITNESSETH THAT pursuant to Section 219 of the *Land Title Act*, and in consideration of the premises, the mutual covenants and agreements contained herein and other good and valuable consideration and the sum of One Dollar (\$1.00) now paid by the City to the Covenantor (the receipt and sufficiency whereof is hereby acknowledged), the parties hereto covenant and agree that the Lands shall not be used or built on except in accordance with this Covenant as follows:

1. In this Agreement, including its Schedules:
 - (a) “development” includes subdivision of the Lands, the construction, alteration or addition to any buildings or other structures on the Lands, the construction and installation of on-site and off-site services [including public roads] that are required for the development and use of the Lands, the installation of landscaping improvements, and the alteration of the Lands in any way including through the deposit or removal of soil, the removal of trees, or through any excavation or regrading of the Lands, and “develop” means to do any of the foregoing, but “development” and “develop” do not include the demolition of any building existing on the Lands as of the date this Agreement is registered in the Land Title Office.
2. The Covenantor covenants and agrees with the City that the Covenantor will not develop the Lands except in accordance with the ERA Design Guidelines that are attached to and form part of this Agreement as Schedule “A” (the “ERA Design Guidelines”).
3. The Covenantor covenants and agrees with the City that the parking facilities constructed on the Lands shall include at least four electric vehicle charging stations that are fully equipped and capable of providing a Level 2 electric vehicle charging level (as defined by SAE International’s J1772 standard), or that otherwise meet the minimum standards for electrical charging stations prescribed under the City’s Off-Street Parking and Loading Bylaw No. 4350 – 1990, as amended or replaced from time to time.
4. IT IS MUTUALLY UNDERSTOOD, agreed and declared by and between the parties hereto that:
 - (a) nothing contained or implied herein shall in any way restrict or abrogate and shall not be deemed to restrict or abrogate, the rights and powers of the City in the exercise of its functions under any public and private statutes, by-laws, orders and regulations, in its absolute discretion, and in accordance with its lawful powers and duties;

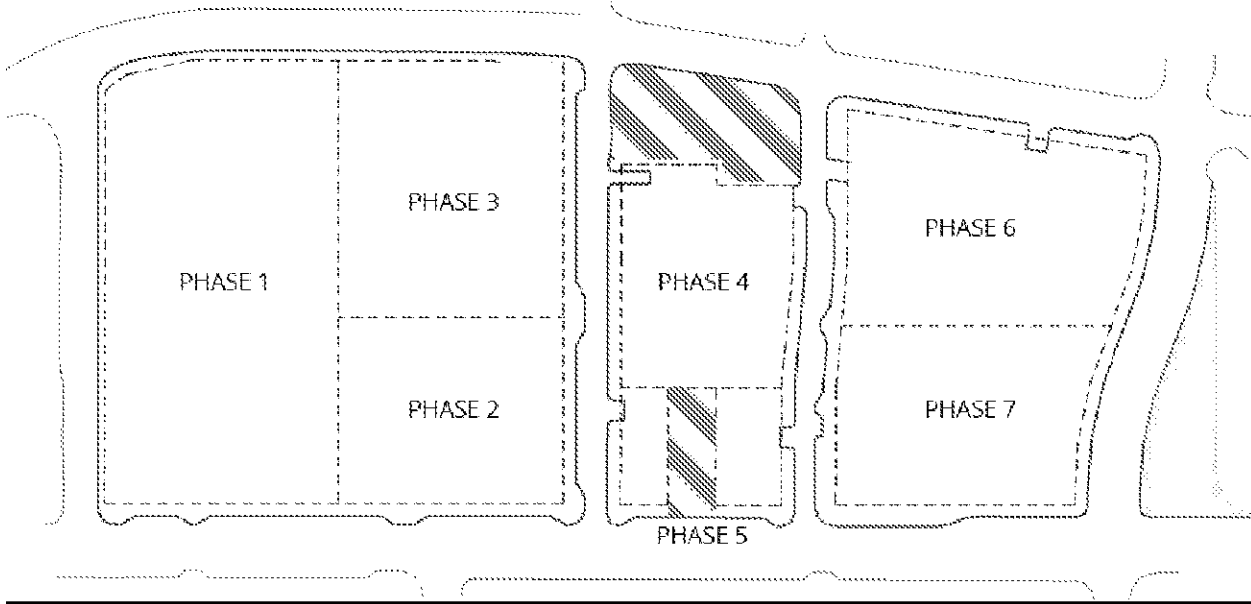
- (b) the burden of the covenants herein provided for shall run with the Lands and will be personal and binding upon the Covenantor during the Covenantor's seisen of or ownership of any interest in the Lands;
- (c) notwithstanding anything to the contrary, the Covenantor shall not be liable under any breach of any covenants and agreements contained herein after the Covenantor ceases to have any further interest in the Lands;
- (d) the Covenantor will deliver, after execution hereof, this Agreement to the City in a form acceptable as a Section 219 Covenant and concurrently such instruments of priority as may be necessary to give this Agreement priority over all financial charges and encumbrances which may have been registered against the title to the Lands at the time of submitting this Agreement for registration in the applicable Land Title Office, save and except those specifically approved in writing by the City or in favour of the City;
- (e) the fee simple estate in and to the Lands will not pass or vest in the City under or by virtue of these presents and the Covenantor may fully use and enjoy the Lands except only for the requirements provided for in this Agreement;
- (f) the Covenantor and its successors and assigns shall at all times indemnify and save harmless the City from and against all claims, demands, actions, suits, loss, costs, fines, penalties, charges, damages and expenses including legal fees and litigation expenses whatsoever which the City may incur, suffer or be put to arising out of or in connection with any breach of any covenant or agreement on the part of the Covenantor contained in this Agreement;
- (g) the covenants and agreements on the part of the Covenantor and herein provided for have been made by the Covenantor as contractual obligations as well as having been made pursuant to Section 219 of the *Land Title Act* RSBC 1996, c. 250 and as such will be binding on the Covenantor;
- (h) nothing herein provided for shall be deemed to constitute waivers of any lawful requirements with which the Covenantor would otherwise be obligated to comply with;
- (i) nothing contained or implied in this Agreement shall prejudice or affect the rights and powers of the City in the exercise of its functions under any public or private statutes, bylaws, orders and regulations, all of which may be fully and effectively exercised in relation to the Lands as if the Agreement had not been executed and delivered by the Covenantor;
- (j) no amendment of, addition to, or discharge of this Agreement shall be binding upon the parties hereto unless it is in writing and executed by the parties hereto;
- (k) if any provision provided for in this Agreement is for any reason held to be invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability will not affect any other provision of this Agreement which shall be construed as if such invalid, illegal, or unenforceable provisions had never been contained therein and such other provisions shall be enforceable to the fullest extent permitted by law;

- (l) the City, in addition to its rights under this Agreement or at law, will be entitled to all equitable remedies, including specific performance, injunction and/or declaratory relief, to enforce its rights under this Agreement;
- (m) the Covenantor shall pay for the preparation and registration, if applicable, of this Agreement together with any concurrent instruments of priority as herein provided for and any amendment, addition or discharge thereof;
- (n) wherever the singular, masculine or neuter is used herein, the same shall be construed as meaning the plural, feminine or the body corporate or politic according to the context in which it is used;
- (o) the parties hereto shall do and cause to be done all things and execute and cause to be executed all documents which may be necessary to give proper effect to the intention of this Agreement; and
- (p) this Agreement shall enure to the benefit of and be binding upon the Covenantor, the City and their respective successors and assigns.

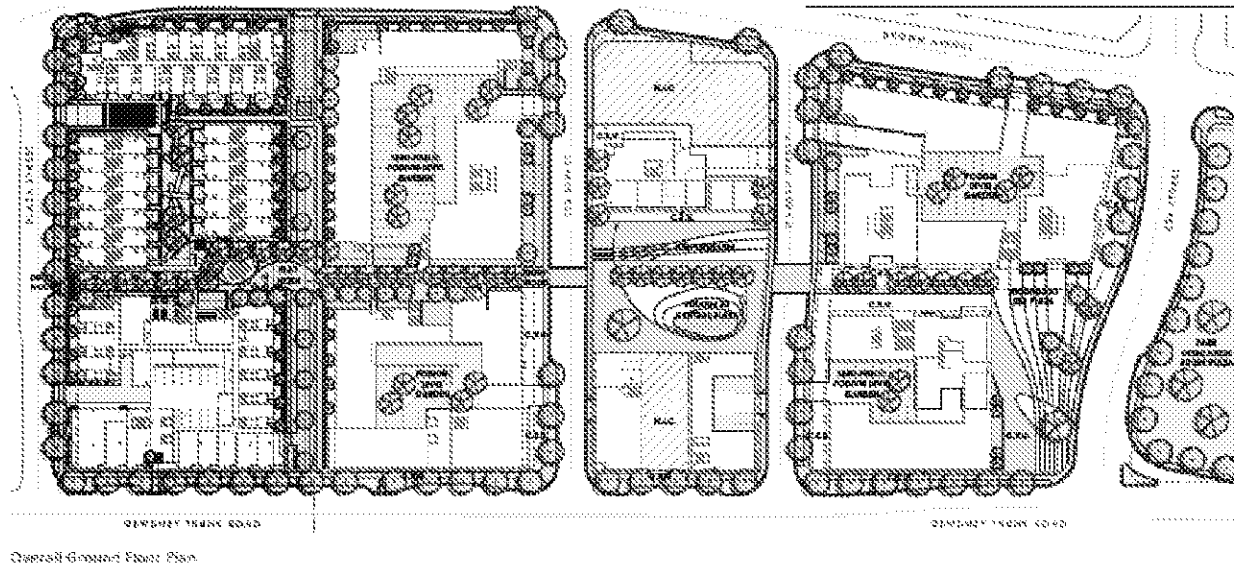
IN WITNESS WHEREOF the parties hereto have executed this Agreement as of the day, month and year first above written.

SCHEDULE "A"

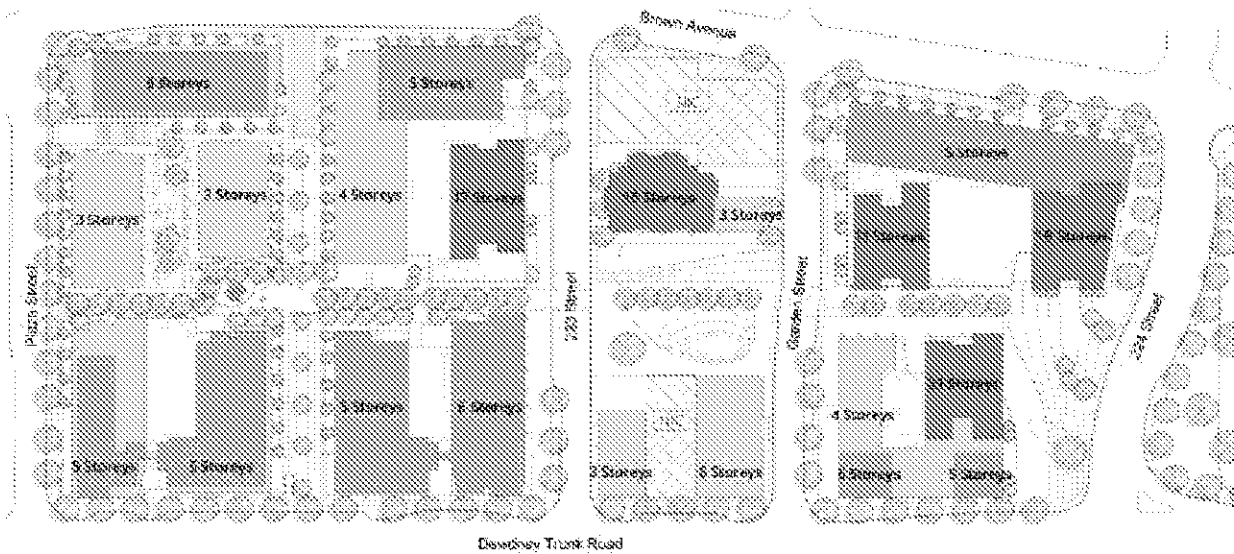
SKETCH PLAN



SCHEDULE "B"



General General Stone Plan



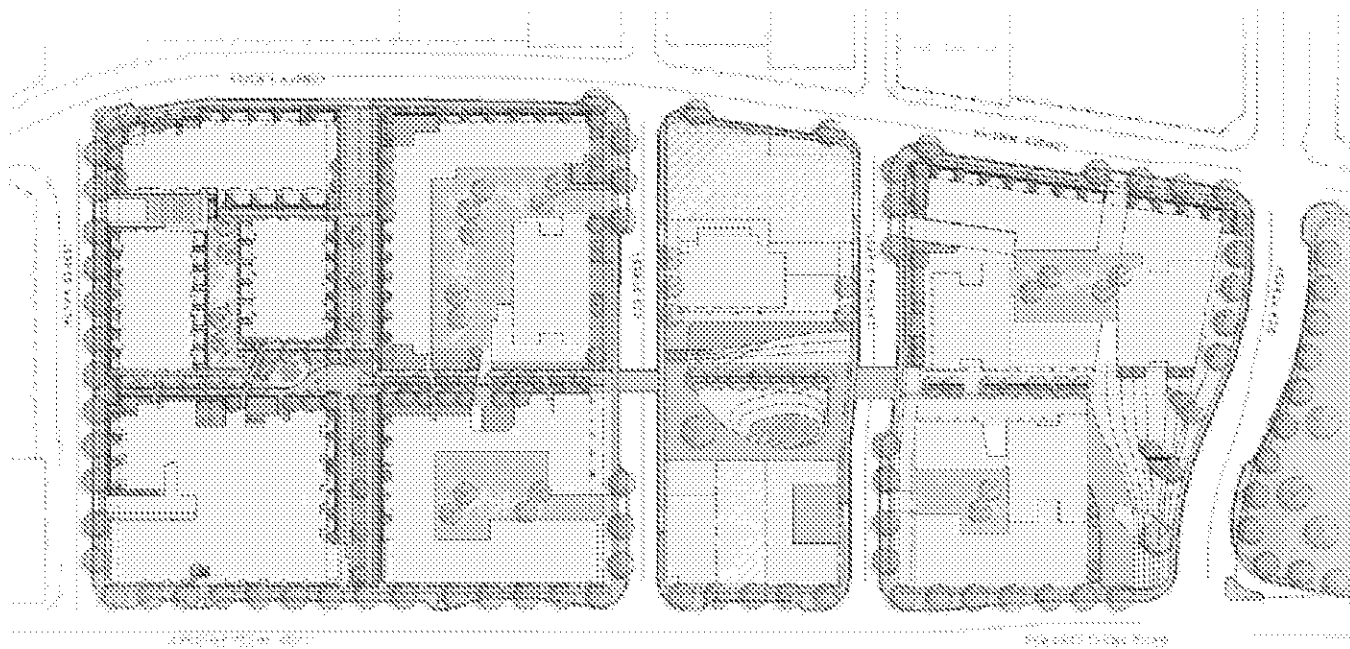
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SCHEDULE "C"

ERA DESIGN GUIDELINES



DESIGN GUIDELINES


IREDALE
 ARCHITECTS

PROLOGUE

The best master development plans are ones that allow for deliberate evolution, a living process that acquires layers of greater depth and complexity as the patterns of use are established. This development is designed for now and for the future.

ERA is conceived as a multi-phased project to be developed over a period of time. These guidelines will set up the patterns to allow ERA and the Maple Ridge Town Centre to positively evolve over the next generation into a mature and unified community.



Figure 1: Aerial View of ERA Development Master Plan

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|--|

1. HOW TO APPLY THESE DESIGN GUIDELINES

1.1 Introduction

The Town Centre Area is a very important part of Maple Ridge and would benefit from a variety of planning and design activities to improve its role as a key community node. To achieve a balanced community, the proposed Master Development Plan (MDP) provides guidelines for the form and character and coordination of common features for the comprehensive development of a 7 phase (2.97 Ha) mixed use development site including commercial, office and multiple residential development increasing the vitality and livability in Maple Ridge Town Centre.

As standards and benchmarks for new developments, these guidelines are informed by best practices in urban design but tailored to the unique conditions of Maple Ridge Town Centre. They will be the basis to guide and shape new buildings and open spaces in conjunction with the OCP Development Permit Area Guidelines to reinforce the objectives of the MDP. At the same time, they provide flexibility within certain parameters to encourage distinction, variety and creative architectural responses.

Where developments do not conform to the guidelines but propose alternative standards, they should be assessed to ensure the intent and spirit of these guidelines are met concerning the Vision, Principles and Intent presented in this document along with the rest of the regulatory applicable documents.

1.2 Application and Intent

1.2.1 The Guidelines: These Design Guidelines, hereinafter referred to as the ERA Design Guidelines (ERA DG) shall be used in conjunction with applicable Maple Ridge Bylaws and the Official Community Plan (OCP) Development Permit Area Guidelines, to guide the development of the master plan proposed 7 phase mixed use development project in the Maple Ridge Town Centre as shown in figure 2. They form part of the No Build Restrictive Covenant on Phase 2 through 7. The objective is to assist in achieving the proposed the Master Development Plan (MDP) for this project. The ERA DG take precedent over the Town Centre Development Permit Area Guidelines for the West Precinct. The Policies contained in the Official Community Plan and the Town Centre Area Plan continue to apply.

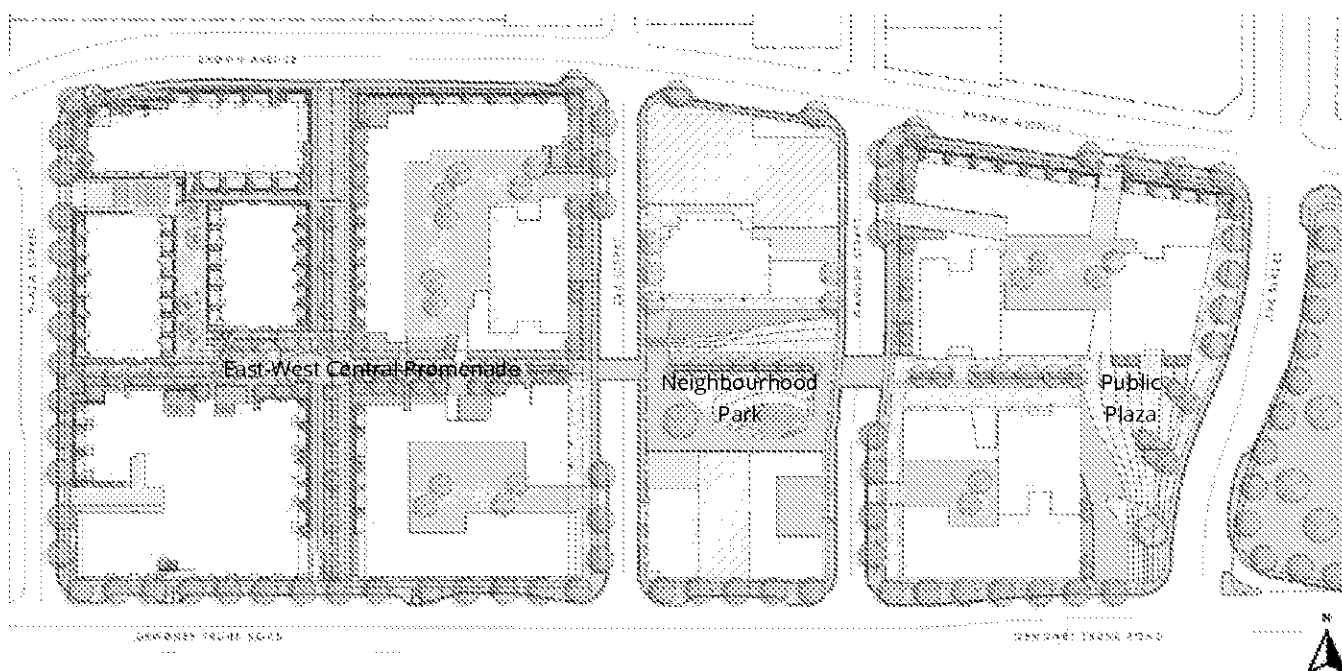


Figure 2: Illustrative ERA Development Master Plan (Building footprints and features shown may vary).

The ERA DG shall be used by the Architects, Landscape Architects and other professionals as the basis for coordinating and achieving a comprehensive development that is seamless across the phases and demonstrating to City staff how each guideline is satisfied.

The purpose is to insure that the key design elements for this project, including: site layout, form and character, public art, stormwater management, sustainability, the pedestrian/public realm, amenities, etc., are coordinated between phases and completed at build out. The ERA DG document will be registered on title in accordance with Council requirements.

1.2.2 Application Process: Unlike other rezoning development applications where the development permits are issued with detailed plans at Final Adoption of the zoning, this development is pre-zoning a seven phase site. Therefore, development permits are to be considered and issued by Council at a future date on a phase by phase basis. Therefore, the ERA DG will take precedence and will be the overarching guidelines. They will be a fundamental tool in interpreting the compliance of future phases with OCP Section 8.11 Town Centre Development Permit (TCDP) Guidelines in terms of form and character Council envisions in the Town Centre Area, within the context of the overall concept and elements to be achieved through this project.

Section 219 Covenant – No Build and Future Servicing sets out requirements for the development permit application process and requirements. These Design Guidelines should be read in conjunction with this Section 219 Covenant.

Future applications for each phase will be subject to the submission requirements and fees in place at the time applications are made. Given the pre-zoned nature of the development site, future applications will require additional supporting reports, plans and details as part of the development permit applications. This will include the following:

1. A Pre-Application Meeting: Before any applications are made, a suitably developed proposal will be submitted and a Pre-Application meeting scheduled with the City. This meeting will include City Department representatives to provide direction to the applicant and their consultants on what plans, information and consultant studies are to be prepared and the exact nature of a development application to be submitted to the Planning Department.

Unless otherwise indicated, the following materials are to be provided one to two months in advance of such a pre-application meeting:

- A. Sufficiently Detailed Architectural and Landscaping Plans: The concept for a phase needs to be developed sufficiently taking into consideration the ERA and OCP Guidelines, including a preliminary analysis of compliance, and the materials in subsequent points.
- B. General Subdivision Information: A full title search with copies of registered documents, preliminary subdivision plans including road dedications, list of anticipated legal documents for the phase, and any related information necessary to provide full context.
- C. Building Code, Fire Code and Other Supporting Materials: All supporting information upon which development of this phase and as has been deferred from the rezoning to the development permit application process, not necessarily finalized, is to be provided.
- D. Transportation Studies: Terms of reference for each phase is to be obtained from the Engineering Department in advance of the pre-application meeting, to better determine the information and analysis required to be included in the pre-application meeting with the City.
- E. Other: Any matters of specifics associated with a given phase that requires consultant studies or other assessments to support the submission for the pre-application meeting.

2. Application Submission and Review: Complete applications will be required to be submitted with the development of each phase, including the items listed below; These items with the full application submission are to have addressed comments and suggestions from the pre-application meeting and include the additional information that may have been identified:

- A. Development Permit Application: The following applies to each application in each phase:
- B. The submission checklist in Schedule D of the Maple Ridge Development Procedures Bylaw No. 5879-1999 for a development permit application will therefore need to be expanded to include additional supporting materials and information for future development permit applications, including those supporting materials and information typically required for rezoning as stated in Schedule C of the same procedural bylaw. A full evaluation against the ERA DG and the applicable Town Centre Development Permit Area Checklists will accompany the submission.
- C. Addressing comments from City Departments and outside agencies, including the School District and the Ministry of Transportation and Infrastructure.
- D. Immediately following the submission of an application, the developer will post one or more Development Information Signs (DIS) with information as required by the City.
- E. The proposal is to be submitted to the Advisory Design Panel for review and comment.
- F. Once the proposed development has addressed all concerns and comments, the developer will undertake notification and will hold a Development Information Meeting (DIM).
- G. Subdivision Application: The submission shall contain the plans, legal documents and consulting reports as may be prescribed by the Approving Officer. It will accompany the Development Permit Application and govern the creation of a parcel to contain a particular phase, and will be subject to requirements and standards in place at the time for road, servicing and other terms and conditions not completed at the rezoning stage. All requirements will be established through a Preliminary Review Letter or similar tool issued by the Approving Officer.
- H. General Subdivision Information: A full title search with copies of registered documents, preliminary subdivision plans including road dedications, list of anticipated legal documents for the phase, and any related information necessary to provide full context.
- I. Sufficiently Detailed Architectural and Landscaping Plans: The concept for a phase needs to be developed sufficiently taking into consideration the ERA and OCP Guidelines, including a preliminary analysis of compliance, and the materials in subsequent points.
- J. Building Code, Fire Code and Other Supporting Materials: These will be provided with the above Applications, as well as other studies, as established through the Pre-application Meeting. Additional requirements or corrections may be required as the projects progress through review toward approval. These will include, but are not be limited to: geotechnical reports, updated traffic or servicing reports, stormwater management strategies specifically integrating Tier A stormwater management into landscaping, market studies, Fraser River Escarpment considerations, code analysis, etc.
- K. Other Applications: These may include development variance permits, tree cutting permits and applications as may be required by other Departments.

Where possible, the above applications and reports would be processed concurrently.

3. Application Coordination: It will be the responsibility of the developer to assign one Coordinating Registered Professional (CRP) to manage and ensure all aspects of each application align and are coordinated. Communications will be through the Principal File Manager for the City and the CRP for the developer.

1.3 ERA Development Vision & Key Design Elements

1.3.1 Development Vision: ERA is envisioned as a well-designed pedestrian, biking and transit-oriented mixed-use development in the heart of the Maple Ridge Town Centre. It will reinforce the existing Civic core with a variety of uses such as recreation and civic spaces, a mix of new residential (in form and tenure), commercial and new employment opportunities, a neighbourhood park with a community gathering space, green building design and infrastructure.

1.3.2 Key design elements:

- A. Residential components of varying scale and density, positioned to maximize daylight penetration into the site and to create visual interest from every corridor.
- B. A prominent east-west public pedestrian open space and pedestrian way from Plaza Street to 224 Street, punctuated with activity areas (including child play, central activity plaza with theatre-like seating, and a major commercial plaza) and linkages to semi public and private space paths and mews and the City sidewalk system.
- C. Mews surrounded by both residential and commercial activity to ensure there are plenty of “eyes on the street” at all hours of the day.
- D. Public plaza in the east portion of the site, lined with commercial shops (inner “shopping row”) and containing significant surface treatment, landscaping, pedestrian amenities, placemaking elements and a public art element.
- E. An attractive and vibrant streetscape along Dewdney Trunk Road with building spaces designed to accommodate ground level shops and services, and access to upper level non-residential components where they occur.
- F. Strategically placed commercial spaces to create an intimately scaled shopping corridor combined with residential use above.
- G. Sustainable design solutions to maximize rooftop space for green houses, gardens and power generation.

The overall design objective is to develop attractive, durable buildings appropriate to the City of Maple Ridge and a public realm that complements the natural beauty of the area, reflecting the policies and objectives outlined in the Official Community Plan while supporting the social, recreational and commercial life in the community.

Future developments, whether they are adjacent or within the community itself, will have this language of patterns to ground their projects and to inform their designs.

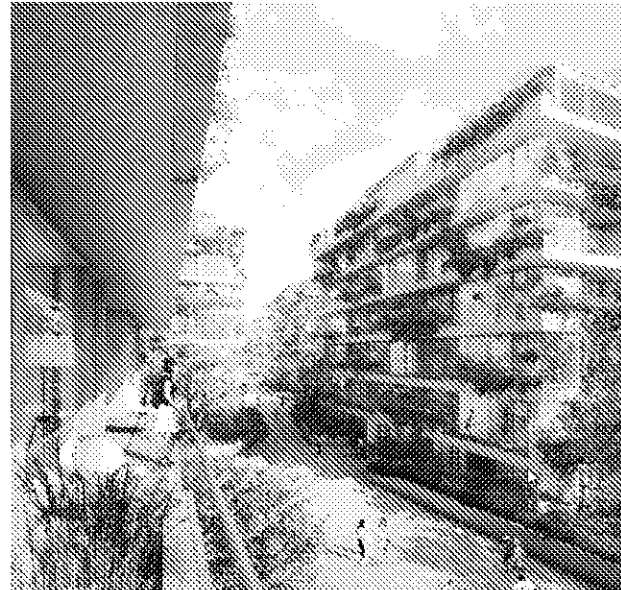


Figure 3: These images demonstrate the possible pattern for a mixed use and vibrant neighborhood complete with public gathering spaces and the important relationship between the buildings and the pedestrian walkways through the site. The buildings should engage the public realm and help define and soften the edges of pedestrian corridors (Representational of character only).

2. OVERALL SITE GUIDELINES

2.1 Design Principles and Purpose

2.1.1 The objective of this document is to provide a design framework for the built form and public realm that provides consistency in neighborhood character, while also allowing for change in site planning and architectural design through each development phase.

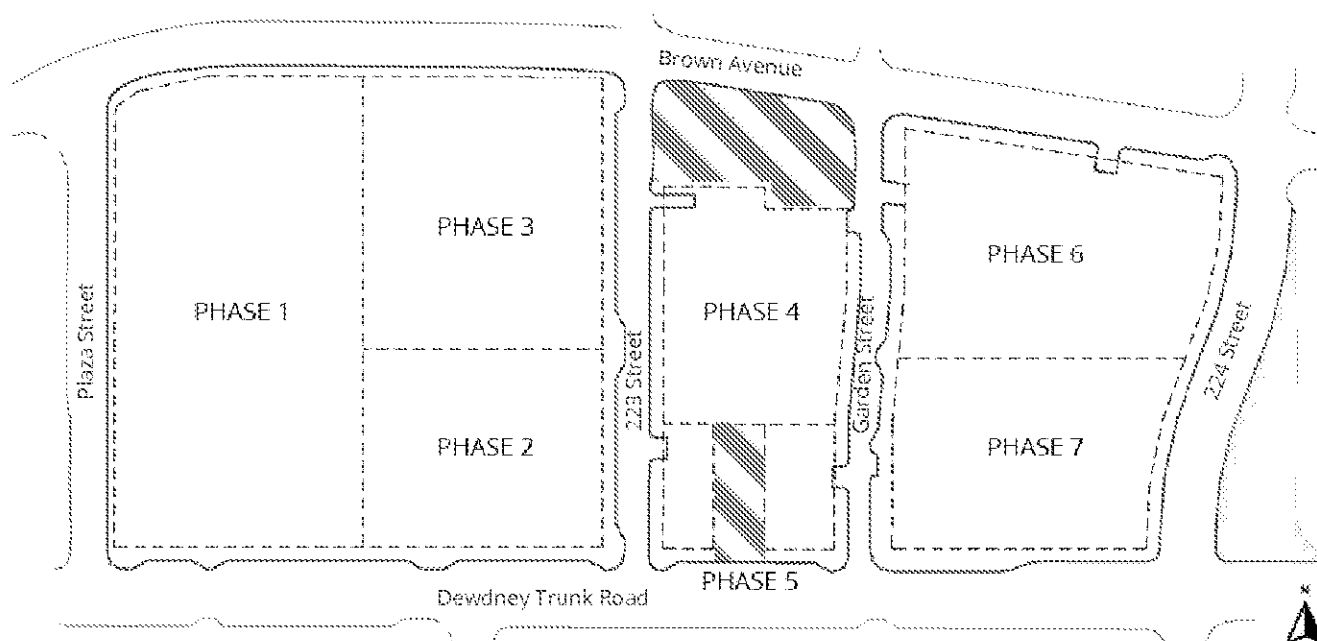


Figure 4: Phasing Plan. The phasing of the development takes into consideration the density for each phase, gradually increasing throughout the phases of the project with the location of the most dense phases (Phase 6 and phase 7) closer to the Civic Core of the city.

2.2 Design Considerations by Phase

2.2.1 Phase 1

High exposure on the north, west and south frontages – Brown, Plaza and Dewdney Trunk – calls for a dignified residential character with the south-west corner marking the entrance to the development along Dewdney Trunk Road and in a way also marking the entrance to the downtown core, more specifically the Downtown West Precinct. As described in section 8.11 of the Town Centre Development Permit Area Guidelines, the goal is to provide enhanced architectural concepts that contribute to pleasing compositions in mass, quality and character; and promote vibrancy of the urban realm. This phase is primarily marked by five and six-storey residential buildings with two groupings of three-storey Ground-Level Apartments in-between. The public green path through the entire site begins here, winding its way between the five-storey and ground-level apartment buildings. Commercial retail units will be located at ground level along Dewdney Trunk Road.

2.2.1.1 Key urban design role:

The south-west corner marks the entrance to the development along Dewdney Trunk Road and in a way also marks the entrance to the downtown core.

2.2.2 Phase 2

This five or six-storey building along Dewdney Trunk Road mirrors the scale and massing of the phase 1 building across the greenway north-south running spine. The building will feature ground-oriented patios

looking out across the spine and across 223rd Street towards the park in phase 4 (See phase 4 for more detailed description of the park). Commercial retail units will be located at ground level along Dewdney Trunk Road.

2.2.2.1 Key urban design role:

This building will form the western edge of the neighbourhood park.

2.2.3 Phase 3

The first tower will be introduced with this phase, accompanied by lower scale four, five or six-storey buildings forming a central courtyard space with the development's amenity space facing out over the central greenway spine to the south.

The tower will range in height from 15 to 17 storeys and it will have the standard 3-meter allowance above for roof articulation and ground-level apartments or an enhanced amenity space for the overall development. The tower should be oriented north-south to help minimize the shadows across the development.

2.2.3.1 Key urban design role:

The tower will help form the north-west corner boundary to the neighbourhood park at the heart of the overall development.

2.2.4 Phase 4

This phase is comprised of a single 10-storey tower and an open green space - neighbourhood park. The neighbourhood park will be the central focus of the development and as such should provide for a variety of different uses and leisure activities. There should be a balance between open lawn space and hardscape surfaces for public gatherings. Some permanent landscape seating should be integrated into the overall design.

2.2.4.1 Key urban design role:

This building will form the northern edge of the neighbourhood park.

2.2.5 Phase 5

Designed to be the only purely commercial phase of the overall site, this phase is comprised of two independent buildings. A two-storey commercial building located at the north-east corner of 223 Street and Dewdney Trunk and a four to six-storey commercial building on the north-west corner of Garden Street and Dewdney Trunk.

The buildings will have high exposure along Dewdney Trunk but will also be expected to properly orient themselves towards the neighbourhood park to the north so as not to turn their backs on it. Special attention will need to be given to engage the pedestrian movement in the east-west direction across the neighbourhood park. These two buildings should be considered as having three front orientations.

2.2.5.1 Key urban design role:

These buildings will form the southern edge of the neighborhood park and because they are both purely commercial in nature, they should actively engage with it. The inclusion of café's or similar type uses will help animate the streets along 223rd, Garden and Dewdney Trunk Road and the neighborhood park to the north.

2.2.6 Phase 6

Flanked by two towers of 11 storeys on the west and 19 storeys on the east, this development has the highest density. Between the two towers is a five-six storey residential building sitting on a common base lined with boutique sized commercial units facing 224 Street and the inner “shopping row” that runs from the central green space to 224 Street.

2.2.6.1 Key urban design role:

This development will form the north-eastern corner of the central outdoor space and also mark the entrance to the boutique shopping row between phase six and seven.

2.2.7 Phase 7

Comprised of the tallest building in the overall development at 21 storeys, this phase also forms the backdrop to the new public plaza located at the north-west corner of 224 street and Dewdney Trunk Road. The base of the buildings will be comprised entirely of commercial retail units to help animate the streets and plaza. Above this will be another four or five storeys of residential development.

2.2.7.1 Key urban design role:

This development will form the eastern edge of the central outdoor space and also have significant exposure to the new public plaza at the north-west corner of 224 street and Dewdney Trunk Road. Special care should be taken to properly address and engage the plaza at ground level with the appropriate scale and articulation of the façade, including the types of CRU's that occupy this space.

Landscaping should play a critical role in the development of this plaza and help define this important outdoor room. Special attention should also be given to the treatment of the paving and ground treatment to ensure this space is appreciated at street level and from above when looking down on it.

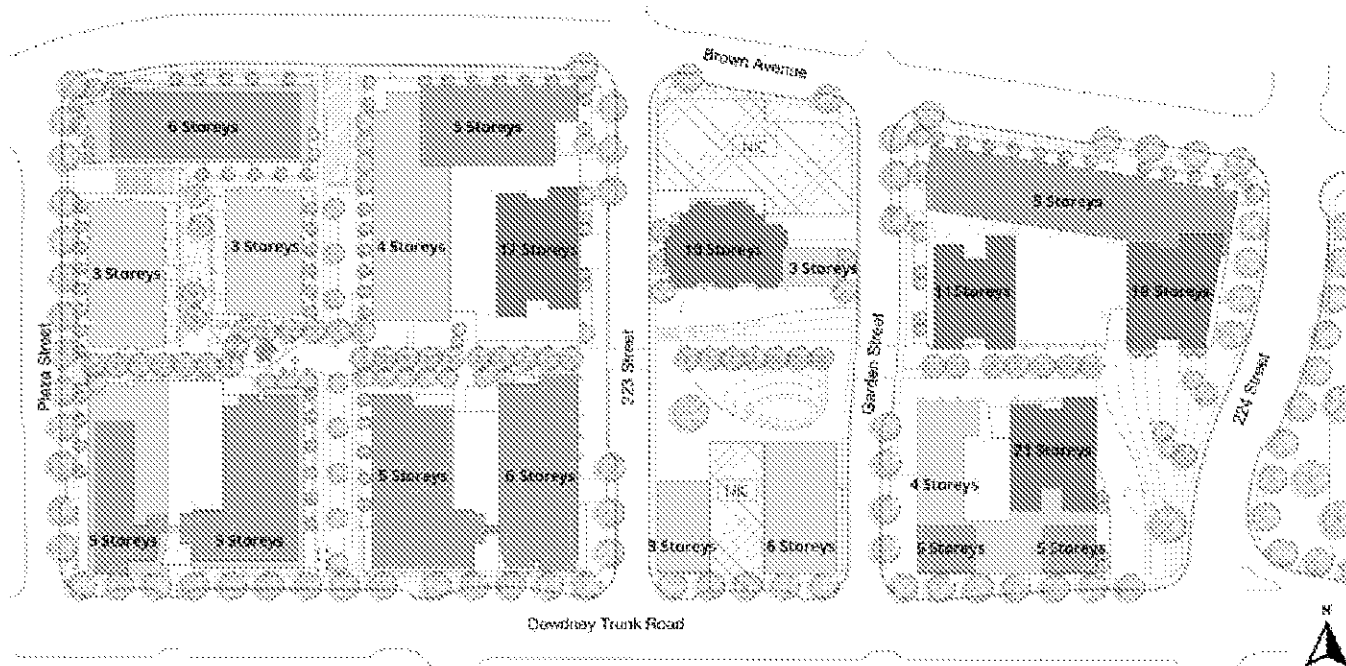


Figure 5: The development gradually increases in density as it expands from west to the east. The densest part of the development is closer to the civic core and culminates in a public plaza (Building footprints may vary).

Parcels not part of this comprehensive development must be taken into account. Efforts will be made to allow for a friendly interface with these properties. In the instance where these parcels become part of the development (or if they become an independent development), they must be integrated well into the overall development and adhered to the ERA DG.

2.3 Key Urban Design Principles

- A. **Pedestrian friendliness**
Streetscapes and public walkways are to be articulated using appropriate scale, character and texture. Street trees, front doors (from ground-level apartments and retail storefronts), different scaled plantings and a variety of paving treatments are to be used to help define these paths and enhance the pedestrian experience. Security is also to be considered through the use of lighting and lower planting along path's edges.
- B. **Public accessibility and permeability**
Provide clearly defined barrier-free access in both North-South and East-West directions through the site. Landscape elements, both soft and hard are to define and enhance these paths.
- C. **Livability**
The overall layout and design of individual buildings and dwellings has been designed to create a variety of different living experiences and to provide a high standard of privacy, usable open space, and recreational amenities. This is achieved through different massing, height and configuration of buildings throughout the various phases of the overall development.
- D. **Neighbourliness**
The form and layout of the site plan was driven by a desire for open space and connectivity. A variety of open and public spaces will facilitate community gathering.
- E. **Diversity**
Diverse housing types including a range of small and large units, condos and ground-level apartments, and low-rise and high-rise buildings will appeal to a large cross-section of the community. Appropriate architectural expression should be given to each as described in other sections of this document.
- F. **Sustainability**
Sustainable design and development principles and practices will be employed such as, water efficient landscape design and rain and storm water management.
- G. **Placemaking**
Public spaces acting as nodes making a positive and memorable contribution to Maple Ridge Town Centre. These spaces should be designed to facilitate a wide variety of civic activities and provide visual interest for the residential buildings surrounding them throughout the use of hardscape, landscape and plazas.



Figure 6: The images above demonstrate a variety of appropriate building typologies for the different scales of residential uses in the development (Representational of character only).

2.4 Pedestrian Pathways and Access Points

These guidelines recommend the emphasis should be given to usable public open space, directly accessible from within a unit. It is strongly encouraged to design common spaces that help maximize the resident's open space experience within the development.

Pedestrian site connectivity and permeability are two of the fundamental objectives of the Policy Statement for ERA, and subsequently play a prominent role in the public open space system for the site.

Access points at the middle of the block as entries are configured to allow strong physical connections and visual corridors to the interior of the site, see figure 7.

In the commercial retail areas, this translates into wider, multi-functional linear public spaces. In the residential areas, the internal mews are reflective of more intimate spaces, while maintaining strong public access to all parts of the site.

There will be a SROW in place for all phases. This has already been put in place for phase 1. Refer to Section 219 Covenant - No Build and Future Servicing for further details.

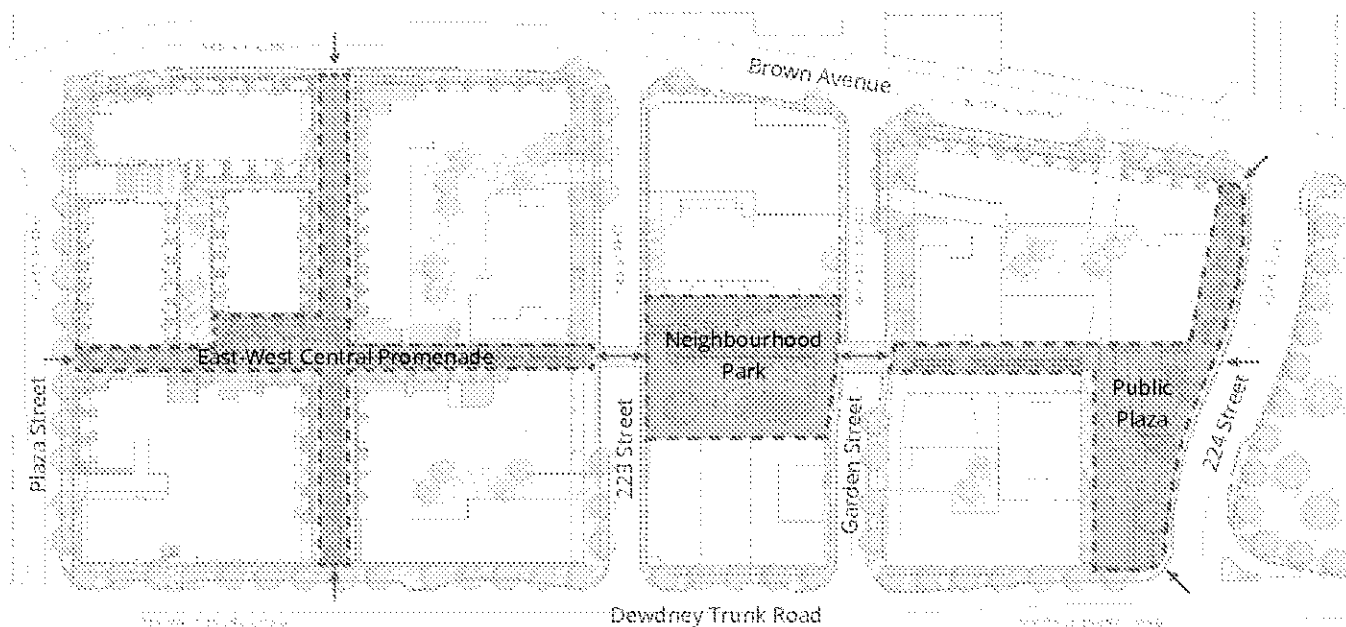


Figure 7: Open space Area and Access Points. Access to public spaces within the development is provided throughout from East-West and North-South (Buildings footprint and features may vary).

2.5 Landscape Design

2.5.1 General considerations

The landscaping should address the form and character of the development through visual form cues from the architecture, while aiming to soften the scale of the architecture in both the primary public pedestrian circulation routes and nodes, as well as the more intimate private amenity spaces.

Taking into consideration the diverse needs of all the stakeholders, and taking cues from the TC Area Plan policies and DP Area guidelines to develop a strong concept for both human activities and the longevity and vibrancy of the integrated natural element. In implementing a holistic approach of "sustainable intensification", these guidelines should provide a range of breathable and enjoyable spaces for those living, working, and visiting this new City Core.

2.5.2 Key Principles

The following minimum general guidelines below should be incorporated to the greatest extent possible:

- A. The redevelopment of the ERA site will feature a mobility network that prioritizes pedestrian circulation and comfort above other transportation modes. Sidewalks and pedestrian street crossings will be continuous, universally accessible, barrier free and clearly designated; Special attention should be paid to crossings at roadways along the East-West Central Promenade; where the pedestrian should be prioritized and protected using an elevated crosswalk (aka. "speed table") with appropriate markings for vehicular traffic & tactile warnings accessible design.
- B. Accessibility Standards and Best Management Practices (BMP) should be applied generously wherever possible in all aspects of landscape design.
- C. In considering the soil conditions of the geotechnical report, the landscape design should maximize soil volumes on-site to increase stormwater detention, prior to infiltration to ground. Additional measures of detention & retention may be required on-site to support the development of each phase. Refer to Rain and Stormwater Best Management Practices Section 2.6.4; using appropriate devices to reduce the impacts of runoff over proposed impervious areas.
- D. Landscape design should take into consideration solar exposure, and appropriately utilize and orient deciduous shade trees to create spaces that are sustainable and useable year-round, while maximizing sunlight wherever possible.
- E. Landscape design should take into consideration the human scale when determining a hierarchy of circulation paths, heights of walls and other elements that delineate programmatic spaces, screening and layering of planting elements, and arrangement of furnishings.
- F. Landscape Design elements should complement the architecture to maintain a cohesive legibility within various 'Character Zones'.
- G. A hierarchy of wayfinding elements (including signage) should be carefully considered and incorporated to allow ease of navigation particularly for those visiting the site, but also as elements that add to the identification and differentiation of various 'Character Zones'. The elements of effective wayfinding being (1) landmarks or other immediate identifiable elements to infer current location and help people to orient themselves, (2) clear routes to destinations, (3) a hierarchy of well defined paths, nodes, and edges of programmed space.
- H. Public art and/or landscape features – particularly that integrate either water or lighting – should be considered at prominent locations to reinforce the importance of the Central Promenade and navigability of the development.
- I. Lighting design should be an integral part of landscape design. While the architecture may provide ambient lighting, the landscape should aim to – at a minimum – provide path lighting along accessible routes. Area lighting may be used as the density increases and the nodes become larger and more commercial/public. Additionally planting may be accented with spotlighting or various other effects to animate spaces that will be used beyond dusk.
- J. Landscape design at the streetscapes should create a continuous and cohesive public realm. Private areas along streetscapes should be vertically separated wherever possible to provide privacy in the instance that the space is actively used, as well as eyes on the street. In these instances, the landscape should still contribute to the visual enhancement of the public realm through use of interesting materials, and ideally, planting material separation.

- K. Walls and fences should be integrated as appropriate where stronger demarcation of private space is to occur. The material used should reflect the Character of the architecture as appropriate to that zone. Consideration of Crime Prevention Through Environmental Design (CPTED) should be forefront when determining heights, with ideal wall heights being 0.61m, with wall-mounted step-lighting at 0.36-0.46m height and a maximum of every 5.0 m on centre unless otherwise noted.
- L. Appropriate soil volumes and drainage layers must be used to ensure the vibrancy of plant material into maturity. A minimum of 0.46m soil depth must be provided to all plants (noting this refers to most shrubs, perennials, and groundcovers without rhizomatous root structures; lawns & extensive green-roof sedums excepted with typical depths of 0.30m), with ideal depth being 0.61-0.76m. All trees require a minimum depth of 0.76m on-slab and an ideal depth of 0.91-1.2m where applicable off-slab; soil may be locally bermed-up to meet this condition. All trees require a minimum soil volume of 10 cbm.; this requirement may be relaxed with discretion in raised planted beds on-slab, where smaller trees may be planted to provide solar shading or ornamental trees planted for aesthetics (noting that these trees may not be counted by the City as "replacement trees" as their mature height & canopy will be limited).

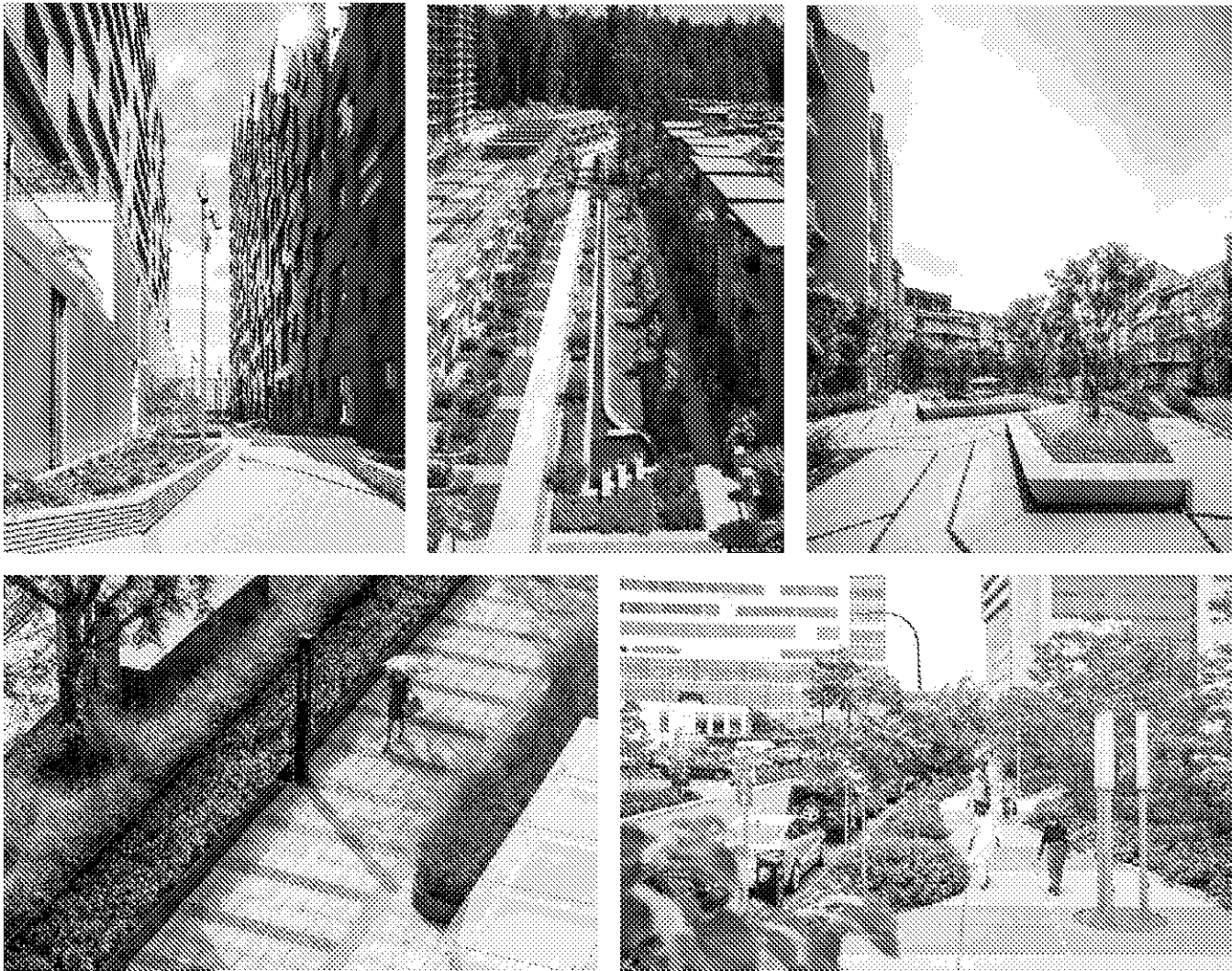


Figure 8: The images above show pedestrian circulation & nodes of various hierarchies, with planting used to soften the edges & appropriately buffer public uses from private uses.

2.5.3 Landscaping and Screening

In Phases 2 – 7:

1. All developed portions of the lot not covered by buildings, structures or paved areas shall be landscaped including the retention of mature trees where feasible. This landscaping shall be maintained.
2. Along the developed sides of the lot which abut a highway, a continuous landscaping strip of not less than 1.5 metres [5 ft.] in width shall be provided within the lot.
3. The boulevard areas of highways abutting a lot shall be seeded or sodded with grass on the side of the highway abutting the lot, except at driveways, and walkways.

2.6 Landscape Sustainability Initiatives

There are several sustainability initiatives incorporated in the proposed landscape and overall site design.

2.6.1 Green Mobility

Given the site location within the downtown core of Maple Ridge the development is well suited for walking, cycling and transit use. These mobility options are expected to attract residents who do not depend on the private motor vehicle.

1. The project rates high on a walkability scale. It is within a 5 minute walk of the civic core, and a 10 minute walk of all of the typical shopping destinations.
2. The site has excellent accessibility to Transit that connects Maple Ridge to adjacent communities.
3. The site offers opportunities for people to live and work within the downtown core and live car free.
4. Resident and visitor bike parking is to be provided on each phase.
5. Bike racks for visitors at street level must be located as per City's by-law and placed in such a way that bikes will not encroach on City's right-of-way when parked.

2.6.2 Sustainable Food Systems

1. Urban agriculture not only contributes to food security but it has a number of other benefits including access to green space, social sustainability and healthy living. For this development we are proposing urban agriculture garden plots and edible planting integrated into the ornamental planting beds.
2. Shared garden plots can best be provided as part of consolidated common outdoor amenity space. Where this is not generally provided, opportunities for gardening could be considered in private outdoor spaces: patios, balconies and roof decks. The incorporation of garden plots should enhance the overall design of that common outdoor amenity and should be considered as one of the variety of programmed uses of those spaces.
3. Urban Agriculture will be provided through the inclusion of rooftop urban agriculture plots for residents. This amenity will include areas dedicated to tool storage, a potting bench and a hose bib(s). These plots help improve the resiliency of local food production, facilitate neighbourly social interaction and contribute to healthy living.

4. Edible Landscaping will be integrated into the townhouse and rooftop garden areas. Where practical, landscape selection will favor plants that also provide food; including fruit bearing bushes that will serve as both ornamental and functional assets.
5. Edible landscaping that may drop fruits and berries, should be conveniently located in high pedestrian traffic areas, fully accessible to residents, and not over public property to ensure that food is harvested without delay and to avoid attracting rodents and pests.

2.6.3 Access to Nature and Open Space

The form and layout of the site plan was driven by a desire for open space, both public and private, and connectivity. The pedestrian walkway not only is a circulation route through the site, but it is the primary connector to all of the open spaces in the development including: semi-private residential courtyards, public children's play and pet-friendly areas, the central park and the public plaza along 224 Street.

These open spaces contribute to the overall greening of the site and access to nature/green space.

2.6.4 Rain and Stormwater Best Management Practices

1. Comprehensive stormwater management plans should be prepared to address both the site and the surrounding area. These should consider existing soil conditions, geotechnical issues, and limitations created by underground parking structures. All Storm Water Management Plans need to be designed to align with current City policies and practices particularly with respect to the 3 tier criteria for Stormwater Management as outlined in the City's Design Criteria Manual (DCM).
2. With consideration of the geotechnical report of the proposed development, existing stormwater infiltration is highly limited on-site due to a high-clay content soil taxonomy, as well as a high-water table. Additionally, the displacement of existing soils in favour of impervious development, further reduces stormwater infiltration. As such, various measures should be aimed at meeting the Stormwater Management Requirements (Section D2 of the DCM). With respect to the ERA development, the following practices will help mitigate small & extended stormwater events.
 - A. Slowing stormwater runoff by maximizing the amount of water detained in new soil volumes on-slab. By increasing the depth of topsoil with a range of 450-762mm as permitted by the design, a majority of the rainwater in typical rain events will be absorbed in the landscape without permeation to-ground/escarpment.
 - B. For rain events with extended intensity or duration, additional systems of detention & retention are required to meet City's drainage requirements. Green Infrastructure & Low Impact Development (LID) solutions should be preferred to engineered detention tanks placed on-site.
 - C. Where stormwater cannot be immediately infiltrated into the landscape or collected for retention, this run-off (from on-site to off-site) should be directed to landscaped boulevards before reaching and further inundating the municipal system. Where streetscape sodded or planted boulevards do not exist, this runoff will be calculated as part of the allowable runoff analysis by Design Engineer to be submitted to the City.
 - D. Where possible, concrete buildings should support & maximize the area of extensive or, preferably, intensive green-roof infrastructure.
 - E. Where possible, the developer may opt to re-use captured/retained rainfall within the building systems, or within the landscape irrigation.
 - F. The Stormwater Management Plan will need to consider the Fraser Escarpment Policies where applicable.
3. Other specific strategies may include green roofs, bio-retention areas, rain gardens and other infiltration features. Permeable paving materials should be utilized where possible to reduce run-off and total load on the stormwater system.

4. Installation of oil/water and soil/grit separators in the storm drain system within parking areas is required.
5. Where possible, rainwater should be directed into small-scale storage and treatment areas located in greenways and other open spaces. Specific strategies – where allowed in reference to geotechnical reports – may include vegetated swales with bioremediation, permeable paving, rain barrels, and underground cisterns to collect rainwater from roofs.
6. From an aesthetic point of view, a distinctive minimal design should be adopted for area drains, catch basins and manhole covers, and used throughout ERA development. This may be an existing set of products or may include unique designs created for this development.

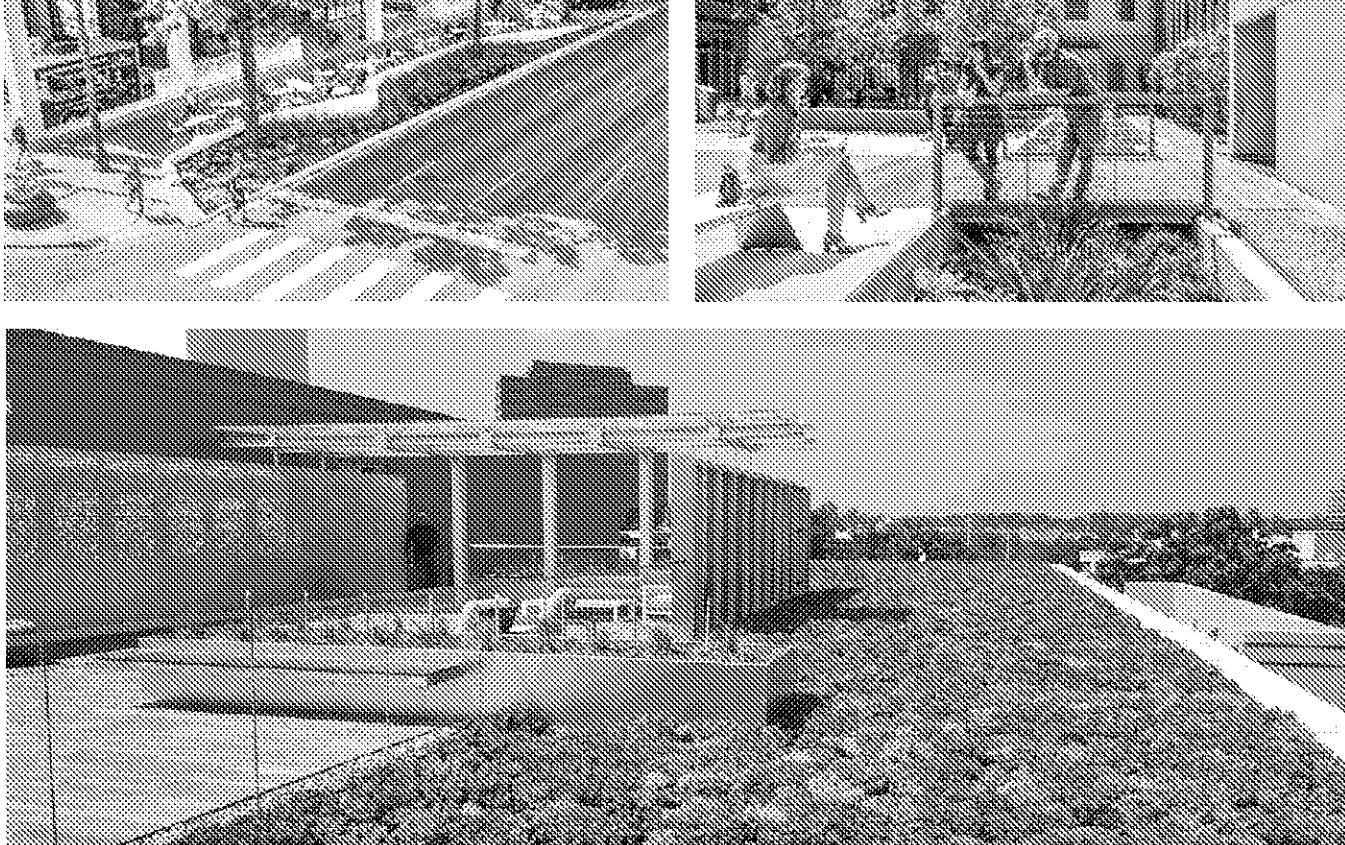


Figure 9: illustrative images above reflect some moderate measures of "green infrastructure" that can support the slowing of stormwater to traditionally engineered detention systems (Representational of character only).

2.6.5 Bird Friendly Design

The landscape design proposes strategies to promote native birds in the urban environment. The proposed landscape plantings incorporate canopy/habitat stratification for a variety of bird species. Plants should be utilized that will attract birds and insects with both native and non-native/non-invasive plant species. Bird houses are also proposed in large shade trees.

2.6.6 Water Efficiency in the Landscape

There are two ways water efficiency is targeted in the landscape.

- A. The landscape planting design should utilize planting that, A) Demonstrates low-water use or drought tolerance; B) Is native or adaptive to local climate and soils to make the lower-maintenance & naturally more drought tolerant; C) Application of mulch topping, to retain any moisture in the soil and even out soil temperature and insulate roots from heat-stress. These planting measures will further reduce water demands for the project.

- B. Irrigation system may be implemented to aid in calculations to attain CoMR Design Criteria "Tier A". It is not required if native/drought-tolerant planting is used throughout, but is strongly recommended for longevity of the landscape design. This irrigation system should be high-efficiency (with smart controls, staggered scheduling, rain-sensors, etc.) and use drip irrigation where possible for added efficiency of delivery to planting.
1. Where possible, the developer may opt to re-use captured/retained rainfall within the building systems, or within the landscape irrigation. Stormwater re-use in irrigation is highly encouraged as a sustainable practice & BMP for stormwater responsibility.

2.7 Green Lane/Mews

2.7.1 For the design of open common spaces, especially mews and/or courts, the following should always be considered:

- A. Orientation - Orient to sunlight and provide good physical and visual access to the sidewalk.
- B. Dimensions - Design as adequate for seating, planting etc., but not so large as to appear barren and uninviting.
- C. Seating - Provide comfortable height and depth, and appropriate arrangement.
- D. Pavement - Use non-glare, non-slip, and safe surface materials.
- E. Trees and Planting - Consider provision for shade and sun. Use to create space and define human scale. Provide protection from wind.
- F. Barrier Free - Provide accessible areas for handicapped.
- G. Amenities - Use pedestrian scaled lighting and other features.
- H. Enclosure - Use landscaping or structure to provide a sense of enclosure, and
- I. Shelter - Use screens and canopies to shelter spaces from wind and rain.

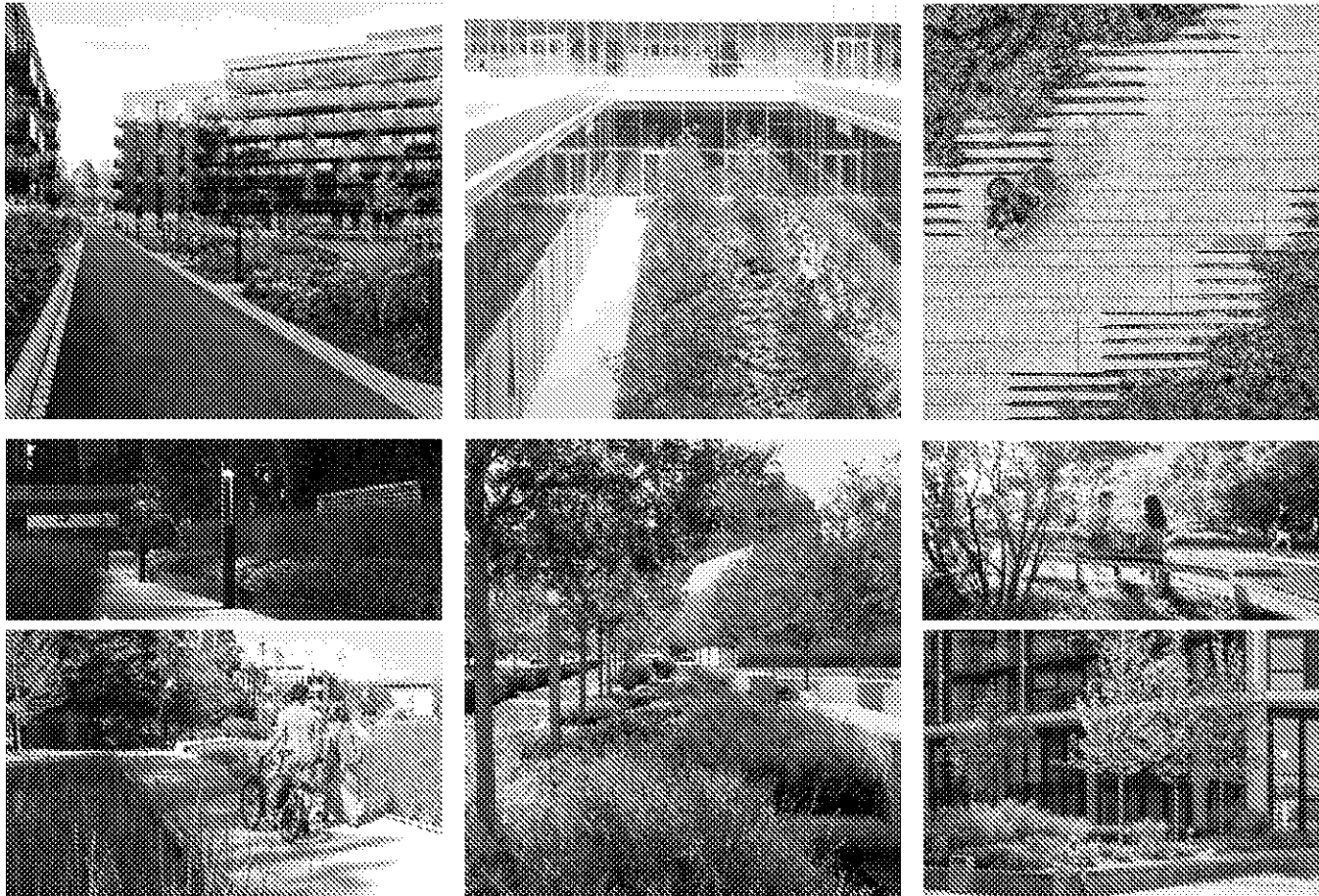


Figure 10: This series of illustrative images shows many dimensions & materials of the larger pedestrian corridors. Ensure that these shared spaces feel inviting to linger in, with seating, lush planting, and clear sightlines (Representational of character only).

2.8 Public Plaza

As the City of Maple Ridge continues to grow, there is an increasing need and demand to revitalize existing parks and open spaces as well as to create new parks and publicly-accessible open spaces.

The new public plaza is one of the main proposed open spaces within ERA. It is envisioned as a part of a pedestrian view corridor that runs across the entire development. The plaza occupies a major location at the intersection of Dewdney Trunk Road and 224 Street and its proximity to the civic core makes it ideal to accommodate future civic events, create a sense of place and become a setting for a variety of uses such as commercial, cultural and public art.

The plaza should visually integrate into the rest of its context and provide a meaningful space in the City's Town Centre. The edges of the square must offer room for restaurants, cafes and retail spaces to spill out into the open space, allowing people to enjoy the views and enhance the experience of the residents and visitors.

Its main goal is to become a major urban gathering point that can be closed to vehicular traffic at specific times for celebrations, exhibitions, festivals, vendors and markets.

Through consideration of this context, the public plaza should fit seamlessly within the City's larger public open space network.

2.8.1 Key design elements

- A. Must be a major portion of the block, with at least two sides open to the street.
- B. Located and oriented to maximize sunlight access throughout the day and provide uses that take advantage of the sunny location (e.g. cafés and patios).
- C. The edges of plazas should be lined with active uses at-grade, including building entrances, to animate and support the open space. Spill-out spaces, such as patios should be encouraged.
- D. At least one edge is open to the public sidewalk. The remaining edges are comprised of building facades (either one building or multiple buildings), potentially with mid-block pedestrian connections.
- E. Must be located at the same grade level as the public sidewalk. Where changes in grade are an important element of the overall design and programming, clear and direct access from the public sidewalk must be accommodated, and ensure universal accessibility.
- F. Should provide continuous weather protection in the form of canopies or arcades at the perimeter of the space.
- G. Plazas may contain a primary building entrance.
- H. Small scale elements should be used to create a human scale, and to define smaller sub-areas within the plaza for ample seating and gathering in the sun and shade.
- I. Creative and dynamic solutions for ample areas of seating should be provided.
- J. Provide pedestrian scale lighting at appropriate locations.

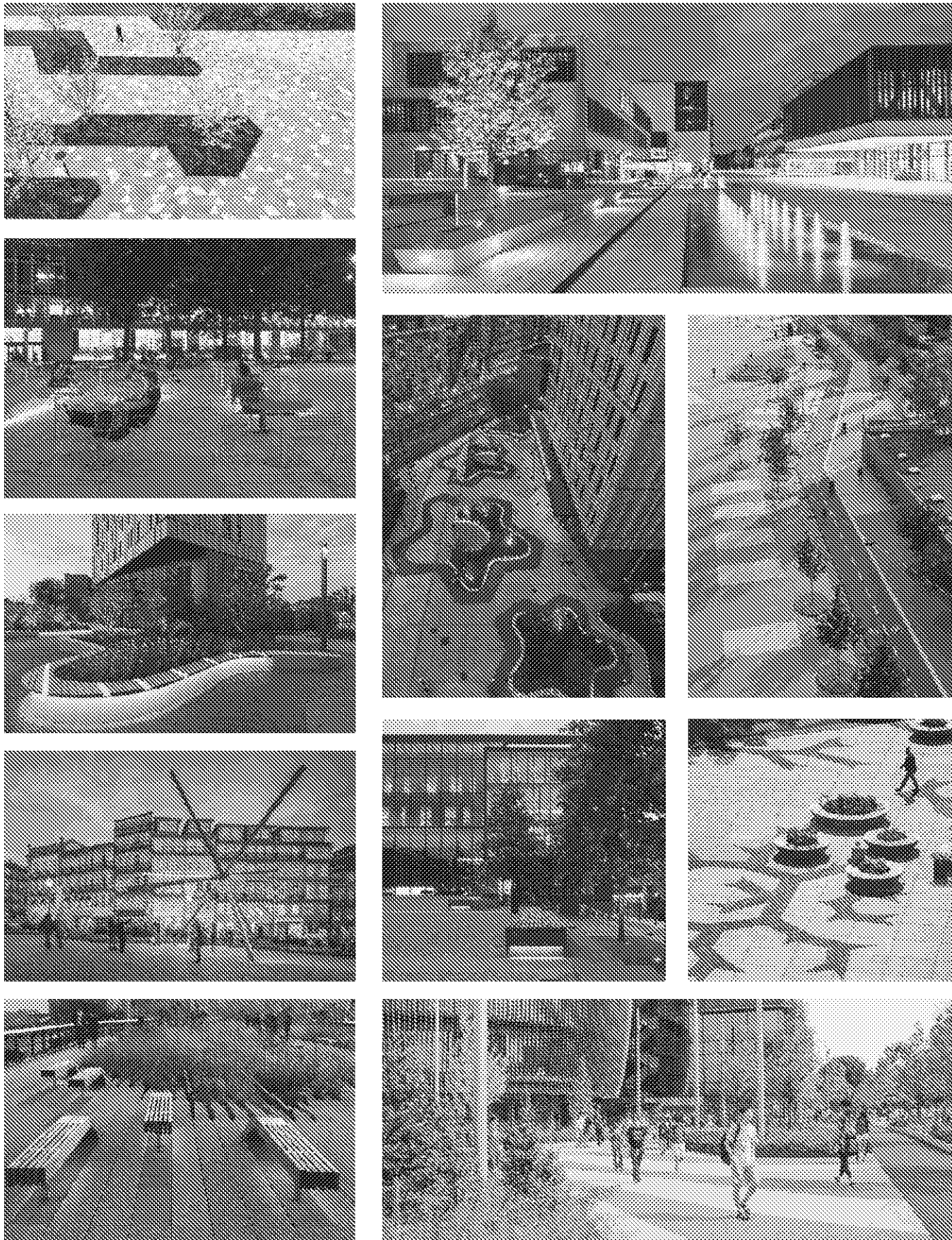


Figure 11: The images above show various approaches to large open spaces, adjacent to the site's higher density buildings. The openness of these spaces prevent a sense of enclosure, and facilitate larger gatherings (Representational of character only).

2.9 Neighbourhood Park

The neighbourhood park offers public open space closely associated with surrounding high-density residential developments. It will provide a communal area for a range of recreational uses including unstructured play, relaxation, and social interaction, totaling an area of 1,910 m² and includes a should include a dedicated storage space of around 25 m².

The park is highly accessible for pedestrians being located central to the ERA development, and central to the major East-West Promenade that engages all proposed buildings. Local streets also draw pedestrians in from Dewdney Trunk Road and adjacent existing neighbourhoods.

2.9.1 Preliminary Neighbourhood Park Guidelines

The park design has yet to be determined but consideration should be made for large flexible seating areas, a play area for small kids, and other uses which support community programming, such as:

1. Large topographical manipulations to create a theatre and amphitheatre that should include a water feature (and possibly public art lighting features) to draw in visitors and drown out nearby noise pollution. Activating this central space will help ensure people are spending time or walking through during the daytime and evening.
2. The edges of the square should allow room for restaurants and cafes at the Office/Future Commercial Units to spill out into the open space.
3. A significant portion of the plaza/park should maintain open paved area to offer the opportunity for a variety of programmed activities such as vendors, performers, exhibitions, outdoor recreation, etc.

2.9.2 Preliminary Play Programming

The ERA Development will provide ample opportunity for a variety of playscapes in each phase of development. The primary goal being play spaces that not only engage toddlers, but children of all ages and physical and cognitive ability.

1. The primary element of 'challenge' refers to the physical and cognitive encounters that a play space provides. The difference between "hazard" and "challenge" must be understood when creating play settings. All due consideration should be taken to adhere to the Safety Guidelines provided by the CSA Group to ensure that the graduated challenges that provide a level of difficulty for all ages, does not create any safety hazards.
2. All play spaces should be softened with planting material that further enhances the space through sensory play & even with the integration of edible plantings.
3. Kids love a challenge, and the following are examples of equipment for testing various abilities at various levels of risk:
 - » Balance Beams
 - » Boulders
 - » Ropes Bridges
 - » Climbing Structures
 - » Platforms
 - » Slides
 - » Sound Makers
 - » Spinners
 - » Splash Pads
 - » Spring Riders
 - » Steppers
 - » Towers and Playhouses



Figure 12: Play should be maximized as much as possible within the defensible enclaves & rooftop-amenities of the lower-density buildings. As the development reaches higher densities, the plazas should incorporate elements to promote play (Representational of character only).

2.10 Semi-Private & Private Residential

Strategically design and locate landscaping to create division between public, semi-private, and private realms. This is achieved through varying heights of plant material, low retaining walls, and fences.

2.10.1 Semi- Private Amenity Gardens

1. Residential patios at podium level will receive additional patio space with raised planters on slab, with hedging and dense planting to screen from semi-private amenity space.
2. Amenity space will include a variety of programming: e.g. open turf space for sport play, relaxing trellised decks for passive activity, children's playgrounds, agricultural plots, etc.
3. Green roofs on podiums and towers.

2.10.2 Private Patios

1. Use layers of landscape moving from shorter plants to taller plants as a transition from the public realms to the private realms.
2. Use landscaping, such as hedging and taller plants, to buffer views into homes and provide privacy for yards and entries.
3. Use low fences, maximum height of 1.0m (3 ft), to define extents of private yards and provide secure areas for residents, particularly along the mews and greenways.



Figure 13: These images show the layering effect that provides a natural, soft buffer between semi-public & private patios. This can be supplemented further by fencing between public & semi-public spaces. (Representational of character only).

3. STREET DESIGN GUIDELINES

3.1 Overall Site Circulation Guidelines

To the extent possible, landscaping along street frontages should coordinate with adjacent street properties to provide a consistent visual corridor.

3.1.1 Cycle Lanes and Pedestrian Paths

These guidelines ensure the provision of adequate facilities for pedestrians, bicycles and transit to promote and facilitate alternative modes of transportation and improve circulation within and around ERA development.

3.1.1.1 The key principles to be applied throughout the development are:

- A. Provide sidewalks within residential projects, connecting from the street or driveway to unit entrances.
- B. Locate buildings to be contiguous wherever possible, and make accommodations for pedestrian circulation between adjacent businesses and sites.
- C. Where new on-street parking is located, provide pedestrian walkways that connect directly to the development.
- D. Provide pedestrian walkways with landscape amenities from within public areas to shop entries, and to other pedestrian oriented uses and destinations.
- E. All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all be designed to allow everyone to enjoy the community and outdoor commercial amenities.

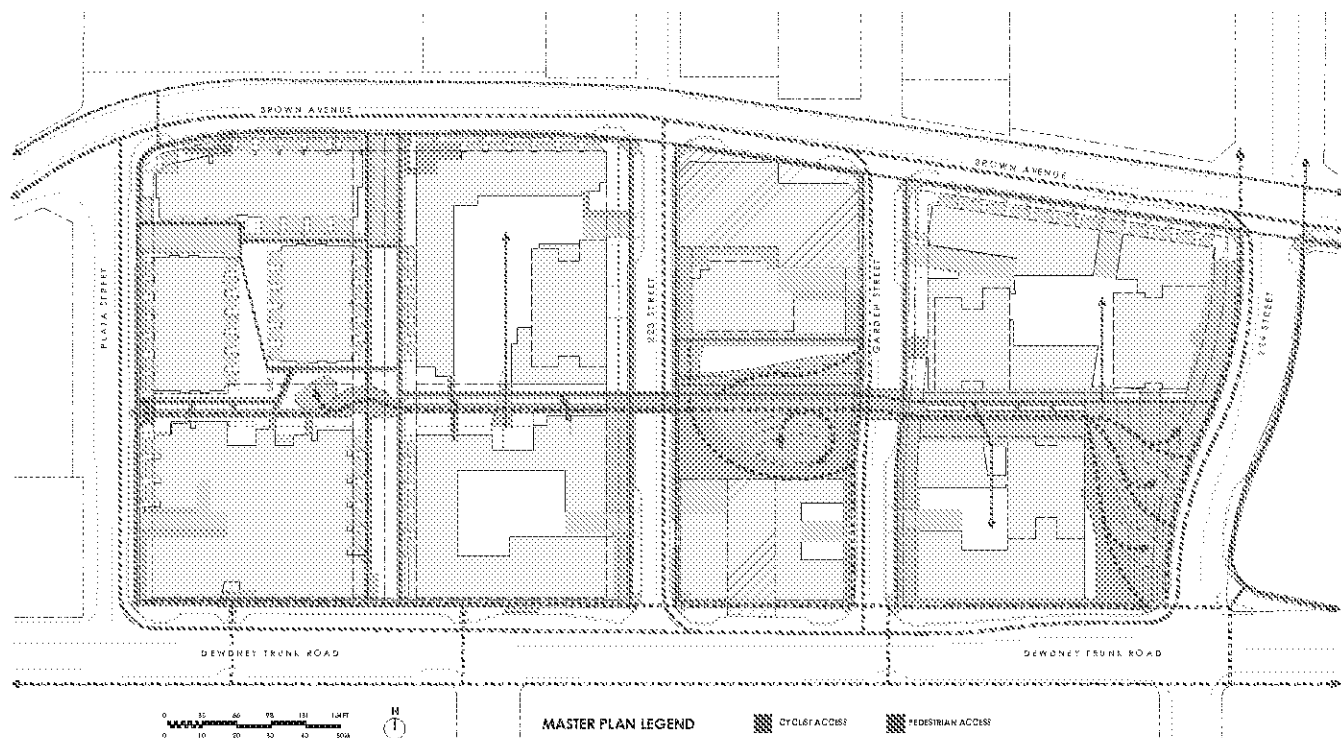


Figure 14: Cycling and Pedestrian Access Plan. Pedestrian walkways of all sizes should seek to create a visual hierarchy through the paving materials & patterns (Building footprints and features shown may vary).

3.1.2 Internal Pedestrian Walkways

Public/off-site right-of-ways and sidewalks should conform to CoMR Design Criteria standards & coordinated design through Servicing Agreements. Internal/on-site pedestrian connections destinations and public spaces should be designed to provide clear orientation and convenient access to residences, offices, commercial destinations and public spaces. Materials and design detailing should reinforce the hierarchy of routes, utilizing variations in both texture and colour.

1. Major routes serving larger numbers of pedestrians should be a minimum of 3.5m wide, and should be constructed with high quality paving materials such as concrete and/or stone pavers. These paths should meet BC Building Code requirements for wheelchair accessibility, either as part of the main route or on an alternate alignment.
2. Secondary routes and those leading to private residences should be a minimum of 1.5m wide, and should be constructed with materials such as concrete and/or stone pavers. Providing safe and comfortable access by individuals with limited mobility.
3. Paving at pedestrian paths should be designed to utilize concrete, stone and/or concrete pavers combined with textured finishes for ramps. The front edge of stair treads should be textured, and/or constructed with a material that visually signals the change in level. In cases where pedestrian and bike paths are adjacent, a separation may be created with roughly textured borders between the two zones.
4. Lighting may be incorporated into or adjacent to stairs and ramps wherever possible.

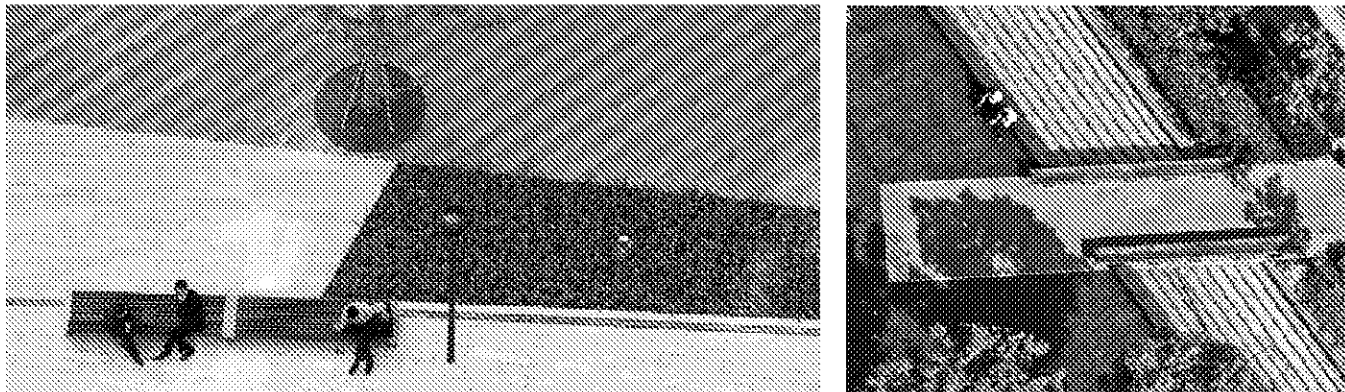


Figure 15: Pedestrian walkways of all sizes should seek to create a visual hierarchy through the paving materials & patterns (Representational of character only).

3.1.3 Accessibility

All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all satisfy the applicable and most up-to-date accessibility requirements and be designed to allow everyone to enjoy the community and outdoor commercial amenities.

3.1.4 Pedestrian Crossings

Streets should provide a pedestrian-scaled environment that calms traffic and allows for a variety of modes of travel. The scale of streets within the development should allow for vehicular access while creating a liveable public open space. Sidewalks should vary in width to create corner bump outs at street corners and mid-block crosswalks only where permitted by the City, with features that increase the safety and liveability of the street.

- Pedestrian Priority
- Special Intersections
- Accessibility
- Lighting

3.1.5 Internal Street Furniture

The concept for the site furnishing is to be modern and minimal so as to not visually clutter the open space, or detract from or clash with the architecture. Rather a family of site furnishing - including benches, social seating, bike racks, waste receptacles, bollards and lighting elements – as complementary to the building design in each 'Character Zone' and consistent with City pedestrian amenity and furniture design and standards.

For any amenities/furnishings on the abutting streets, the downtown standard site furnishing from the CoMR are required to be followed. These standards might change from time to time and they need to be in line with the standards required at the time of the application. The current standards are as follows:

- A. Tree grates – Dobney Foundry, Ivy Pattern, bare metal finish
- B. Benches – Victory Stanley RB-28 9 with arm rest in middle
- C. Garbage – Victory Standley SD-35
 - » side door opening – City to provide lock
 - » black recyclable cage/ basket option

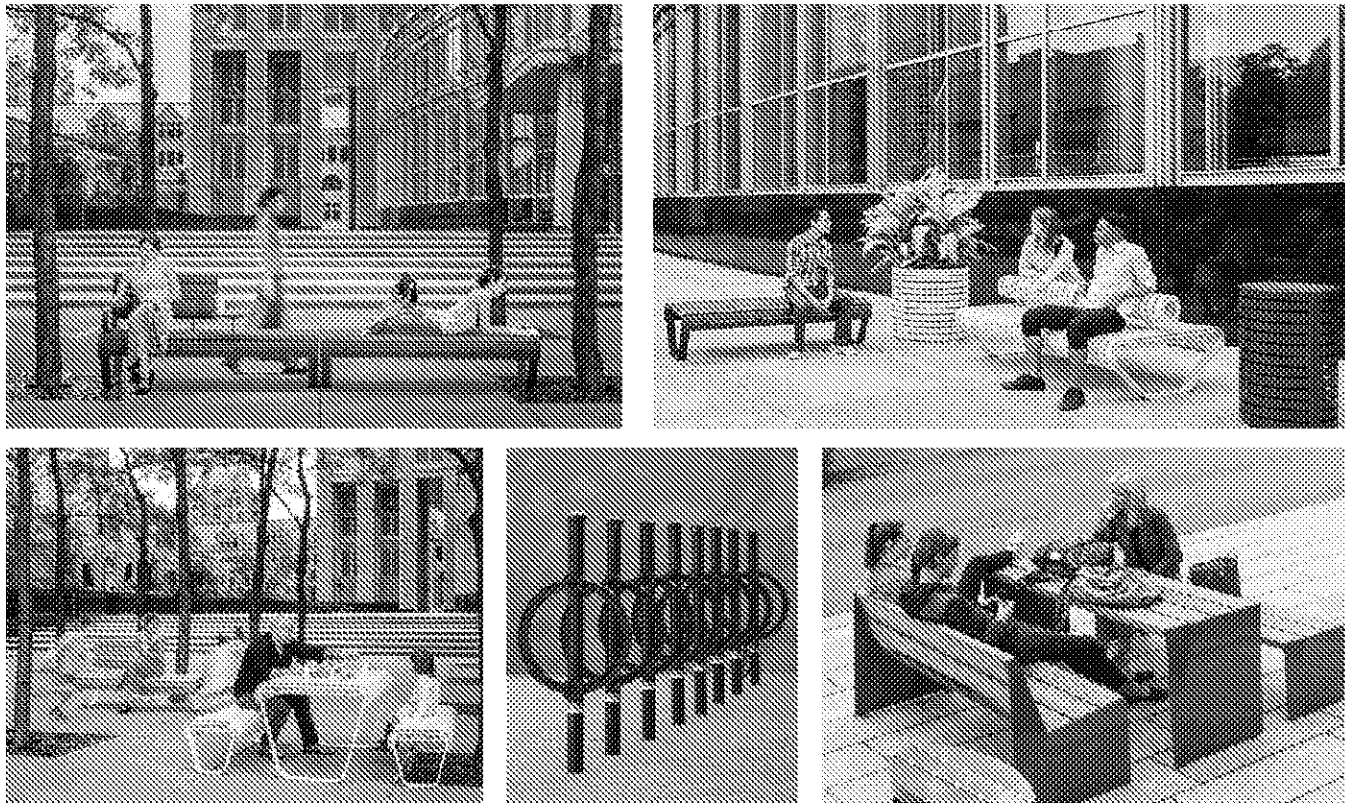


Figure 16: The images above show a variety of contemporary site furnishings that can take on the materiality & colouring of the surrounding building architecture, to create a cohesive palette (Representational of character only). Offsite furnishing should match existing CoMR standards.

3.1.6 Transit Shelters

If the provision of a transit shelter is required, provide transit shelter that enhances the streetscape and that offers adequate seating and shade.

The Developer should consider Translink/Coast Mountain Bus Company's employer and findings of the applicable local Traffic Study. Translink/Coast Mountain Bus Company should be consulted at an early stage of the development planning process in order to ensure that adequate service is provided and sufficient provision is made for transit routes, stops and shelters, primarily along Dewdney Trunk Road.

3.1.7 Public Art

Public art not only animates a space, it provides wayfinding opportunities and designates a meeting place, landmark, or memory to increase the awareness of place. Commercial areas, especially at the corner of major downtown intersections, are ideal places to provide public art. Public art can also be weaved into the fabric of the east-west pedestrian spine, street frontages and the Plaza to embody specific community themes and commemorative elements.

Art can add humour and, if done at an appropriate scale (and is durable, colourful, and constructed with care) it can add to the marketability of a commercial development. The use of water, ponds, and fountains will also aid in the buffering of traffic noise from 224 Street by adding an acceptable white noise to the space.

In the final phase, an appropriate selection process with the City will be established to select an artist and the major art work to be installed by the developer in the Plaza.

Appropriate lighting of the artwork shall be considered to add another dimension to the experience.



Figure 17: Public art within this development should follow the appropriate selection process. The thematic examples above show large, iconic, or interactive works indicative of what may be implemented (Representational of character only).

3.1.8 Internal Signage

As noted in section 2.5.2 Landscape Design - Key Principles G; the three elements of effective wayfinding should be followed. It is essential to establish hierarchy of signs that follow a signature theme throughout ERA Development to ensure that the design of signage for each landmark, node, and path, references this theme.

Signs should either physically relate directly to the features they represent or be grouped to create a comprehensive wayfinding system.



Figure 18: These images show signage in a variety of sizes & materials, to improve wayfinding through the site. A signage consultant may be involved to create a cohesive hierarchy throughout the development (Representational of character only).

3.1.9 Street Lighting

3.1.9.1 Lighting for Comfort

While street lighting will ultimately be dictated by Civil Engineering requirements, the choice and location of site furnishings and lighting should emphasize safety, comfort and quality, and should reflect a consistent contemporary character for ERA as noted in 'Street Furniture'. Select and locate lighting fixtures to meet CPTED design principles. City streetlights should be 4000K.

See <http://www.designcentreforcpted.org> for more information.

3.1.9.2 Lighting Distribution

CPTED principles invite more lighting wherever possible, however, impacts of lighting on adjacent areas to the immediate programmatic space, should be considered. Illuminating Engineering Society of North America (IESNA) Practice Manual notes that site lighting should not shine onto neighbouring property. Furthermore, exterior lighting fixtures that use luminaires with 1000 lumens or more should be shielded, and those with over 3500 lumens should meet the Full Cutoff Classification as described by the IESNA.



Figure 19: This series of illustrative images shows a variety of lighting applications. From area & pedestrian lighting, to accent & feature lighting. Lighting increases sense of safety and should be a primary focus of the urban design. (Representational of character only).

3.1.9.3 Reduction Of Light Dispersal

Efforts should be undertaken to reduce light pollution such as limiting excessive use of decorative lighting on the exterior of buildings, ensure light fixtures are oriented downwards and are designed to light wall or ground surfaces, reduce glare effects by generally avoiding the direct exposure of light sources (indirect instead of direct lighting), etc.

3.1.10 Tree and Vegetation

Trees and shrubs should provide a green structure and context throughout the development, with the intensification of buildings heading east, mimicked in the density and size of tree and shrub species chosen. Consideration of solar exposure, prevailing winds and wind tunnels, and other microclimate elements in relation to the specific program of each open space, should be considered in determining an appropriate plant palette.

3.1.10.1 Key design principles

- A. While unobstructed lines of sight must be maintained at intersections, crosswalks and entrances to individual buildings, screening elements for unsightly service areas and utilities should be provided.
- B. Tree placement should be determined in relation to other landscape features to define public, semi-private and private space, to direct and enhance views, and to soften street edges.
- C. Landscape material should use arrangements of spaces to create outdoor spaces that explicitly address the feeling of safety.
- D. Along internal roads, install street trees 8-10m on centre with minor variations to suit entry driveways and site utilities. Trees are to be a minimum of 7cm caliper. Trees along public streets are determined by City of Maple Ridge.

3.1.10.2 Suggested tree species

The following list are suggested species of trees to be used in the ERA development:

1. *Acer circinatum* (Vine Maple)
2. *Acer glabrum* var. *douglasii* (Douglas Maple)
3. *Acer rubrum* (Red Maple)
4. *Cornus nuttallii* (Pacific Dogwood)
5. *Fagus crenata* (Japanese Beech)
6. *Fraxinus angustifolia* 'Raywood' (Raywood Ash)
7. *Gleditsia triacanthos* (Honey Locust)
8. *Liquidambar styraciflua* (American Sweetgum)
9. *Magnolia* spp. (Magnolia)
10. *Picea omorika* (Serbian Spruce)
11. *Pseudotsuga menziesii* (Douglas Fir)
12. *Quercus palustris* (Pin Oak)
13. *Robinia pseudocacia* (Black Locust)
14. *Thuja plicata* (Western Red Cedar)



Figure 20: Vegetation is applied in many ways throughout the site; and should be maximized as much as possible for sustainable practice, aesthetic appeal, and restorative function (Representational of character only).

3.2 Specific Street Guidelines

The following streetscape guidelines address each street interface and respond to specific existing or proposed uses on neighbouring properties.

3.2.1 Dewdney Trunk Road

Generous pedestrian walkways at the Commercial Retail Units (CRU) are to be designed to allow CRU to spill-out towards streets, creating friction/interaction with pedestrians. Architectural cover to provide rain protection and engagement along the storefront facades.

1. Public/off-site right-of-ways and sidewalks should conform to City's Design Criteria standards and coordinated design through Servicing Agreements.
2. Typical: A 1.2m wide exposed aggregate concrete boulevard strip will contain:
 - A. Street trees in decorative metal tree grates ("Dobney Foundry, Ivy-48CAT" pattern, bare metal finish), in a continuous soil cell trench as required for sufficient volume (10 cbm/tree);
 - B. Site furnishings, including benches ("Victory Stanley RB-28", with centre arm rest, Black), garbage receptacle ("Victory Stanley SD-35", side door opening c/w lock provided by City, Black, and option for recyclable cage/basket) or as directed by the City.
 - C. Street & pedestrian lighting standards.
 - D. A 2.0m wide broom finished commuter sidewalk will be adjacent to the property line. Street parallel parking
3. Additional to the off-site streetscape design, Plaza Street will have casual surveillance from residential patios that are vertically separated from the street for privacy.

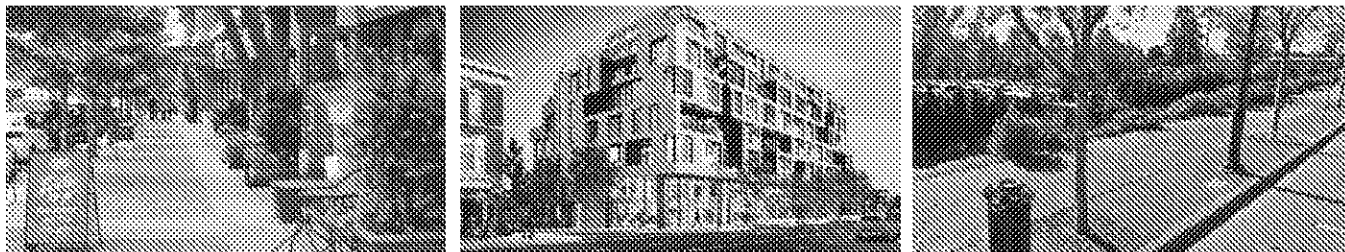


Figure 21: The images above show an overall approach to streetscape design; with a mix of commercial and street-facing residential units. (Representational of character only).

3.2.2 Brown Avenue

Casual surveillance from Residential patios vertically separated from street for privacy.

1. Street trees will be part of a continuous lawn boulevard
2. On street parking
3. A multi-use path for cyclists and pedestrians
4. Boulevards either side of Avenue

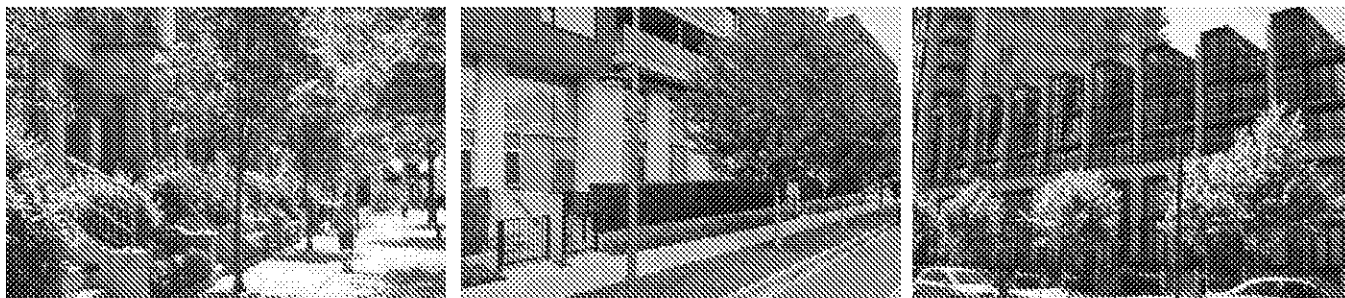


Figure 22: The images above show an overall approach to urban design; with an emphasis of quieter, ground-oriented residential units. (Representational of character only).

3.2.3 Mews / Pedestrian Way

Transition from raised residential private patios, to at-grade barrier free public space.

1. 2.4m (8') wide pedestrian path
2. Grass shoulders expand path surface to 4m wide to accommodate fire truck access
3. Residential patios either side of path are raised to provide 'eyes on the street'
4. Live work units (more urban in nature) adjacent Dewdney Trunk Road



Figure 23: The images above show an overall approach to urban design; with an emphasis on wider, but quiet, shared public corridors. (Representational of character only).

3.2.4 223 Street / Garden Street

Collector thoroughfares for pedestrians. Pedestrian-oriented with measures to reduce vehicular speed. The future of Garden street will be subject to review by the City and may be eliminated.

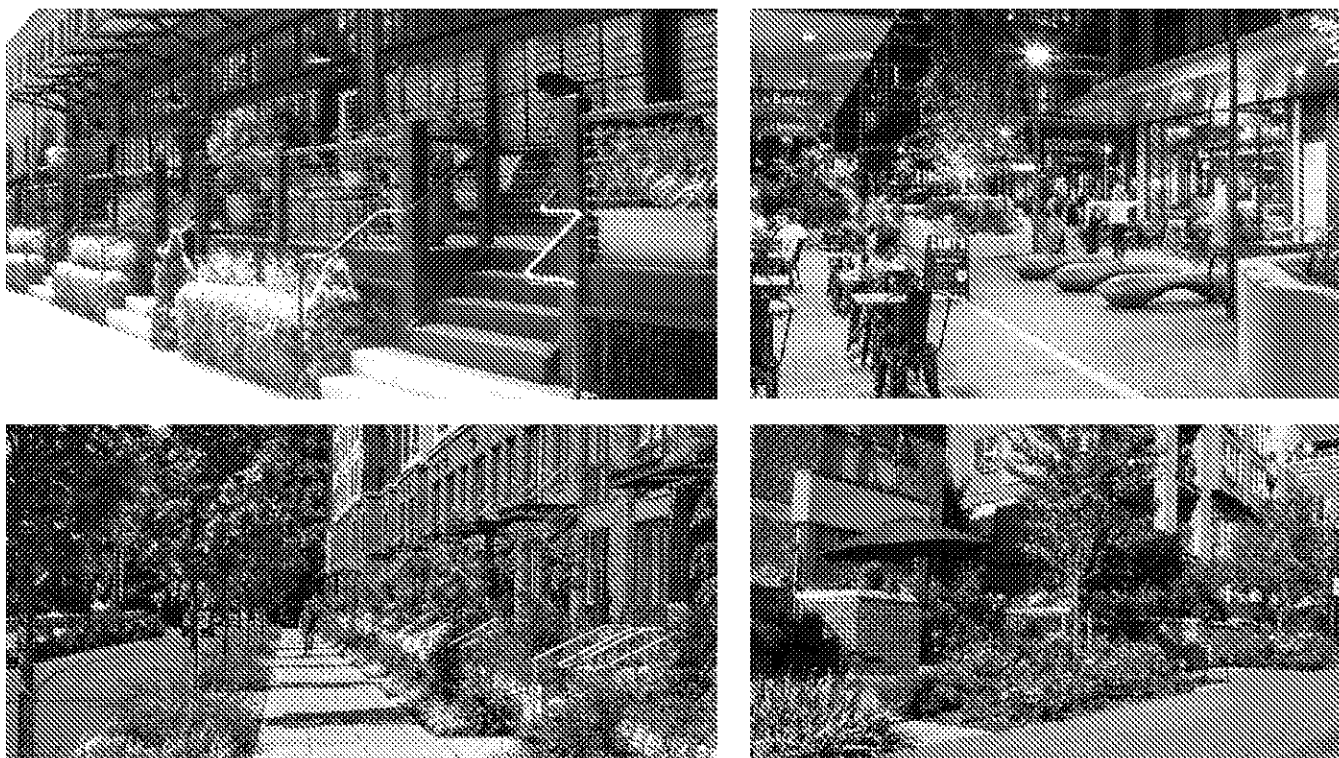


Figure 24: The images above show an overall approach to a more live-work urban form; with smaller commercial-residential units partially separated from the public realm. (Representational of character only).

3.2.5 224 Street and New Public Corner Plaza

The realignment of 224th Street was driven by the desire to improve a problematic intersection at Dewdney Trunk Road and improve the effectiveness of the vehicular circulation.

It was also proposed to create a more substantial public plaza in keeping with the OCP and designating this area as a new urban gathering point for Maple Ridge.

Pedestrian first orientation to be the entry and extension to the public plaza.

1. 224 Street is realigned thus creating an open space on the east side
2. Built form is pulled back from the street to create an open space that spans the street
3. Paving treatment may extend across the street (contingent on additional study and coordination with the City) and could be closed for festivals. Plaza and pedestrian path to the east could accommodate a farmer's market and/or potential location of public art.

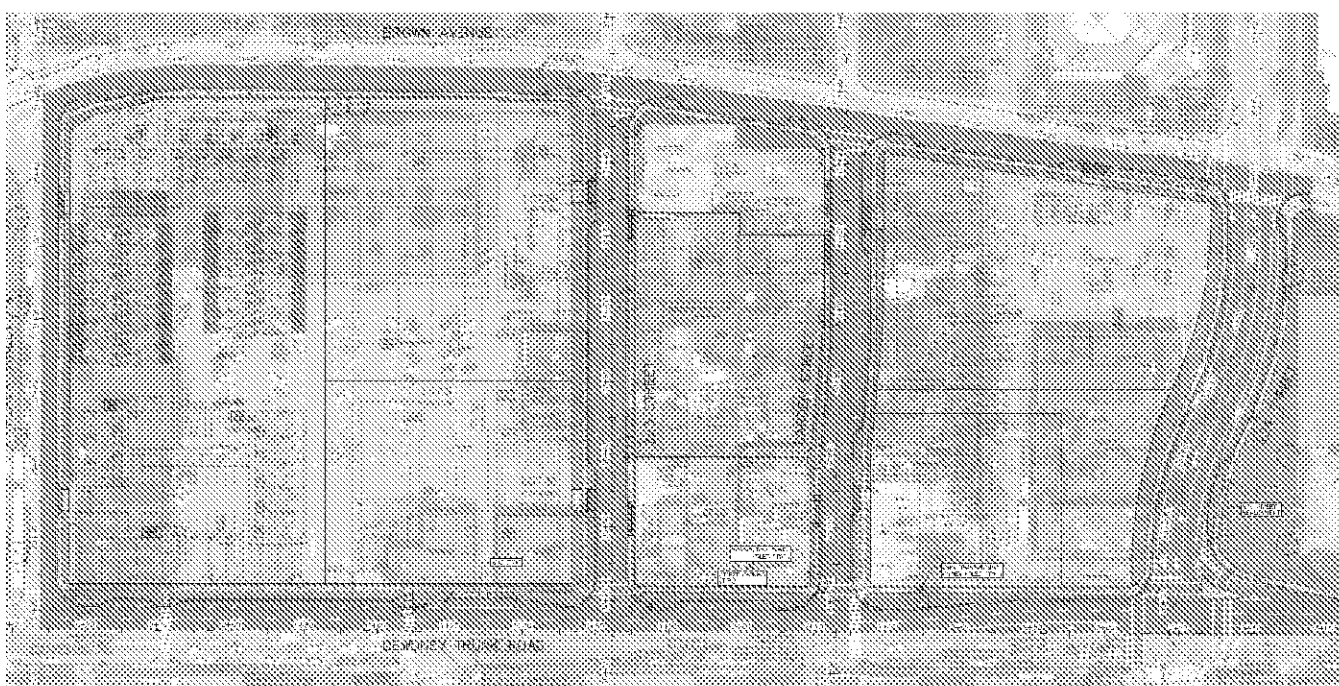


Figure 25: The intersection of 224 Street and Dewdney Trunk Road promises to become a major urban gathering point that can be closed to traffic at specific times of year for celebrations and civic events.

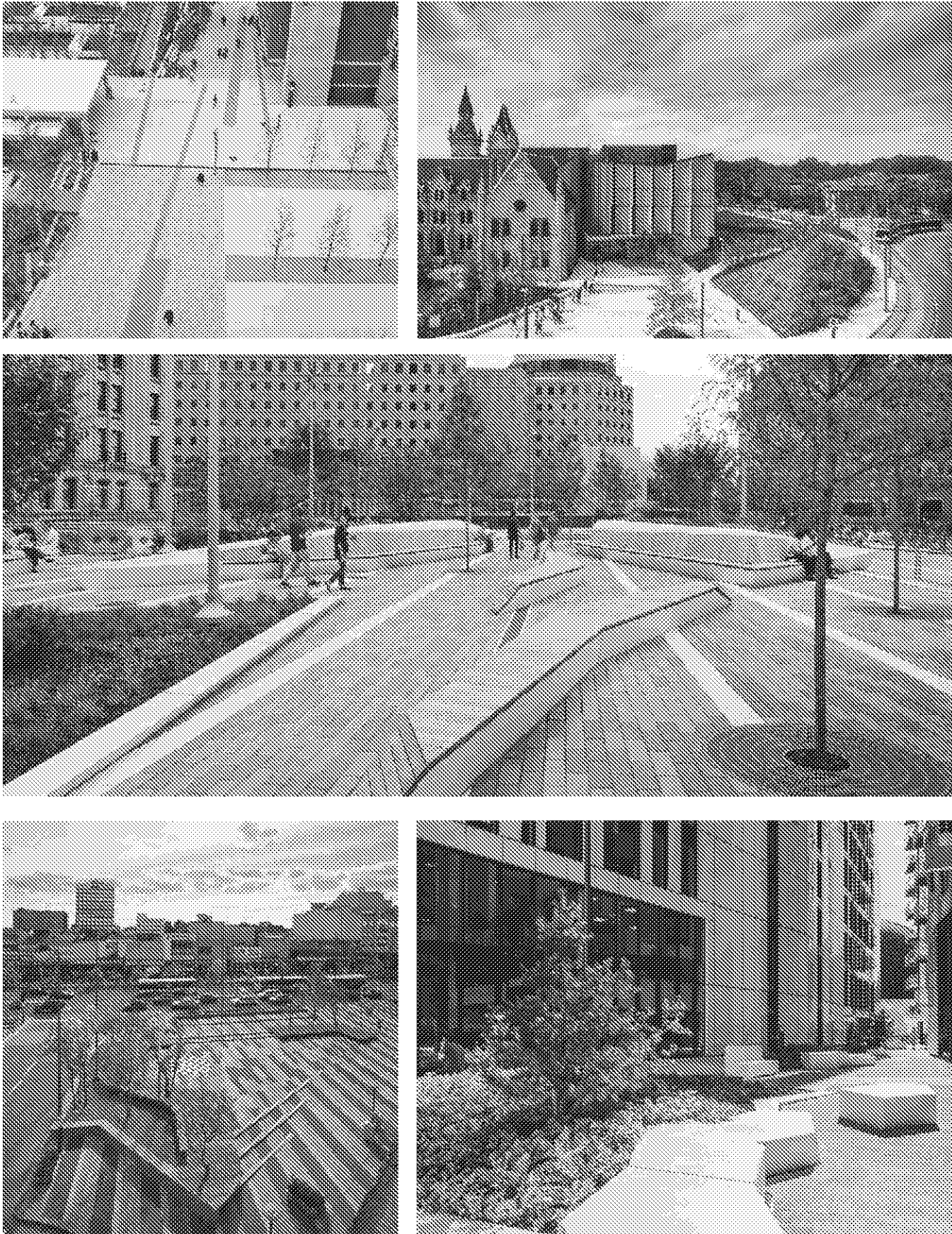


Figure 26: The images above show an overall approach to a more urban form; with large open public plaza for congregation & events (Representational of character only).

3.2.6 East-West Promenade

3.2.6.1 At Residential

1. 2.4m (8') wide
2. Heavily planted
3. Finer-grained in residential scale & character
4. Places to sit and gather
5. Incorporation of rain water infiltration

3.2.6.2 At Neighbourhood

1. Path widens at public park/plaza
2. Incorporation of extensive tree canopy
3. Plaza space to south (adj. Commercial/Office) is multi-use
park space to north is a neighbourhood & community destination
4. Park has berm to create interest & play opportunity on a flat site
5. Incorporation of rain-water trench & water feature in plaza

3.2.6.3 At 224 Street

1. Path widens to 3m (10')
2. Single row of large trees rain-water trench takes on a more urban character more hardscape
& urban in nature adjacent to commercial



Figure 27: This series of images shows the potential programming of the nodes along the east-west central promenade (Representational of character only).

4. BUILDING DESIGN GUIDELINES

4.1 Massing and Building Form

As the existing context has not yet incorporated the principles of the OCP, this master plan strives to create an appropriate response through the seven different phases. To mark the progression through the site, the massing, density and height of the buildings increases gradually as they get closer to the intersection of Dewdney Trunk Road and 224 street, the most urban part of the site and home to a new civic core of Maple Ridge.

- A. Throughout the development, medium-sized buildings are typically stepped back above the first 3-5 storeys of the building creating a lower podium that reduces the overall mass of the buildings at street level. This design solution defines the difference of uses within the building, such as residential units over retail and office spaces.
- B. Street walls (building faces directly adjacent to a street) should be between three and six storeys in height. If less than three storeys they are usually not well defined, and over six storeys they can be oppressive to most people. In most instances, street walls should step back above the third to fifth floor.
- C. The point towers should strive to achieve a height to width ratio of no less than 2:1 to 3:1 for the 21-storey building. When not possible, the massing of the building should encourage visual verticality through key architectural moves.
- D. The podium should be at least two but not more than five storeys high. This morphology permits small-scale retail storefronts on the commercial streets. This duality of commercial and residential creates a strong street presence, with the tower providing residential density that enlivens the neighbourhood and makes the local commercial activity economically viable.
- E. This form of building can also accommodate two storeys of ground-level apartments above the podium level without creating an overwhelming street front as long as the upper units are setback from the cornice line.
- F. A clear difference between the entrances to the towers and the residential units should be maintained throughout the project, keeping the street fronts for retail and residential units.

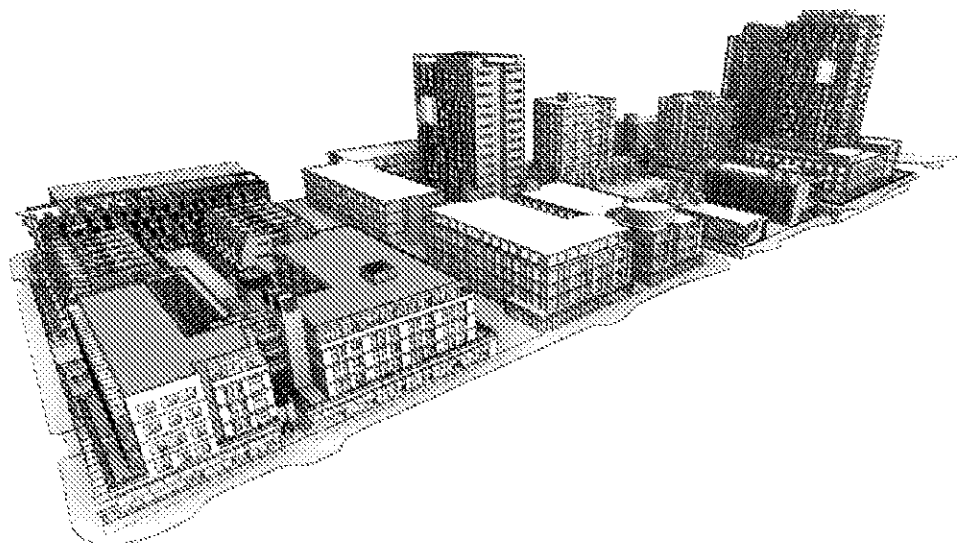


Figure 28: The varied heights give the development character and create visual interest. The residential towers are strategically placed on the most eastern side of the site to reduce the shade over the community park and to provide a visual dynamic for the development (Representational of character only).

4.2 Height

The master plan incorporates a variety of building heights, with the tallest being – around 21 storeys – located in the southeast of the site, in close proximity to the proposed new public plaza. Tower buildings decrease in height to the west creating an overall skyline of decreasing height transition across the site.

Mid-rise buildings range in height from 4 to 6 stories along Dewdney Trunk Road and Brown Avenue.

The heights have been intentionally varied to create a dynamic profile across the development. The towers are staggered to provide good site lines for the units and ensure that maximum sunlight penetrates deep into the site.



Figure 29: Upper floors on 5-storey buildings should be stepped back to assist in reducing the massing. Corners at intersections may deviate from this pattern to help establish a stronger street presence in these locations.

Taller buildings are offset from one another to limit views from one building to the other. The staggering arrangement, along with the north-south orientation of these buildings also help mitigate shadows into the site.



Figure 30: Master Plan Shadow Study. Taller buildings are strategically placed to minimize shadow impact throughout the site (Building footprints may vary).

4.3 Setbacks

Several setbacks to roads are increased in order to accommodate green pedestrian passage and to accommodate green infrastructure and/or to provide sufficient space to allow larger trees to grow in the years to come.

- A. The typical residential setback is between 3 to 6 meters (10-12 ft), which is sufficient to provide an outdoor patio or stoop that can accommodate outdoor living and/or seating.
- B. Other commercial or retail areas have a minimum 0.6m (24 inch) setback along Dewdney Trunk Road which will increase the overall width of the sidewalk zone, while allowing for outdoor seating, signage or other requirements, while not impacting the path of travel on public sidewalks.
- C. A setback between each phase of approximately 7.5 to 18.3m (25-60 ft) shall be maintained, to accommodate sufficient breathing room between buildings and with significant landscaping or other landscape features.
- D. Lastly, an approximate 19.2m (63 ft) setback is provided on the east side of Phase 7 to ensure sufficient area to create a civic gathering space that will extend across the street, integrating the development into the rest of its context.

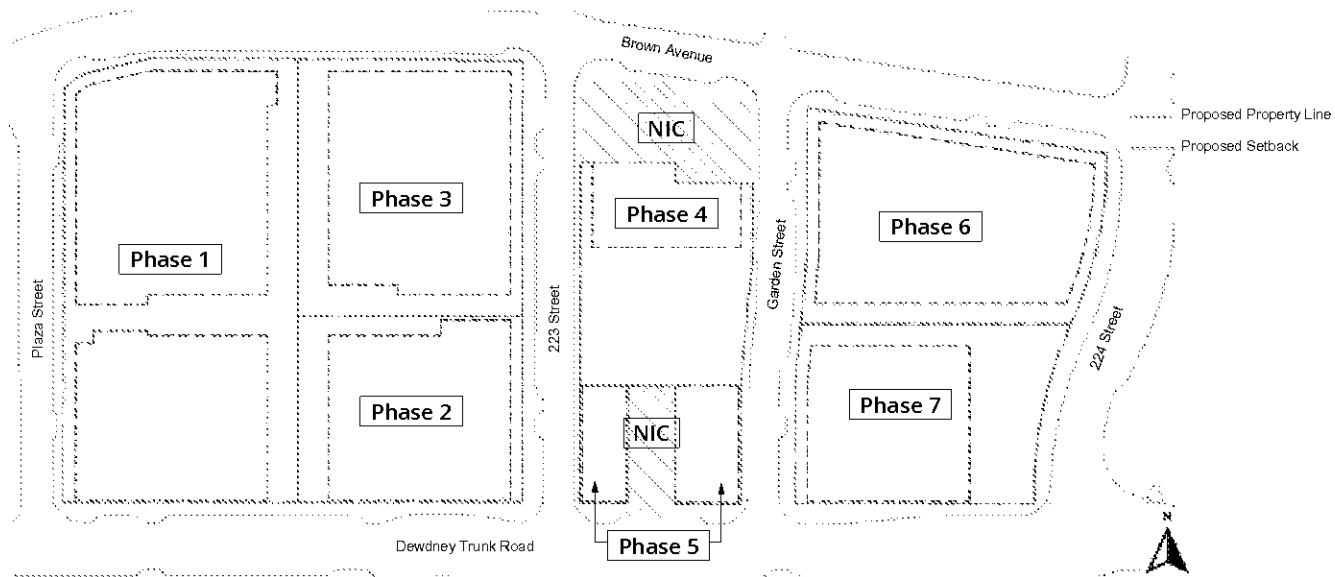


Figure 31: Master Plan Setbacks. The established setbacks on each phase outline the overall space required to create vibrant links between public and private areas.

4.4 Garbage / Recycling Staging Areas

4.4.1 Internal Storage

The solid waste and recycling storage in a building must be accessible to all occupants of the development, and be clean, well lit, and conveniently located for users to get access to, including those with restricted mobility. The location of such a room or rooms is preferred to be within the underground portion of a building, such as an underground parking level, permitting convenient staging on refuse collect day.

4.4.2 Staging Areas

Especially for multi-unit residential buildings, if containers are stored underground, they will likely need to be relocated above ground for servicing on collection day (by building staff or a container pullout service provider). Thus, a temporary storage area should be incorporated in the ground level portion of

the building and site plans provided for collection vehicles to manoeuvre and to collect materials without impeding street, parking entrances and sidewalk circulation (ensuring no height clearance issues).

4.4.3 Other Design Considerations:

- A. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces.
- B. Both internal and staging areas are to comply fully with the City Waste Collection policies.
- C. Any portion of the staging area at grade must be within private property sufficiently setback from streets and pedestrian areas.
- D. Loading areas, garbage containers and recycling containers shall be screened from any adjacent residential lot or from overlook from upper storeys, to a height of at least 2.5 metres [8 ft.] by buildings, a landscaping screen, a solid decorative fence, or a combination thereof.

4.5 Building Sustainability Strategies

As these guidelines are further developed in City policy, these will be reflected in every phase. In the phases where it is mandated, these will apply and may take the form of:

- 1. Increased envelope thermal performance (both walls and fenestration),
- 2. Increased energy efficiency of mechanical systems such as the inclusion of HRV's,
- 3. Inclusion of more passive strategies such as window shading on the applicable orientations,
- 4. Green roofs
- 5. Use of low or classified Zero-VOC construction materials and finishes,
- 6. Re-Use, Recycle or Use Rapidly Renewable materials
- 7. Choose Third-Party Certified Products, etc.

4.6 Parking Considerations For All Phases

4.6.1 All vehicle parking is to be provided in a below grade or above ground parking structure.

4.6.2 For above ground parking structures, the design of these parking structures should blend in with architectural elements of the main commercial or residential building, including the use of architectural or decorative grille or other similar elements for wall openings. Steel mesh is not recommended.

4.6.3 Parking provided is to be separated between commercial and office use parking spaces (i.e. non-residential parking) and residential parking spaces (i.e. parking for residents and parking for their visitors).

4.6.4 There will be a security gate between the non-residential and residential parking. The security system will be designed to allow access to visitor's parking spaces by visitors, but retaining security to the portion of the parking spaces for residents should be recommended where the overall building configuration makes it possible.

4.6.5 Residential parking shall separate parking for visitors and parking for residents.

4.6.6 A gate into the parking area from the street level is optional as determined by circulation and security considerations.

4.6.7 Pedestrian access, including those with restricted mobility, will be designed to provide safe and convenient access between the separate parking areas and the portion of the building the parking is intended to serve. Access from residential parking areas will take into consideration families with children and individuals using mobility assistance equipment.

4.7 Building Typologies

4.7.1 Multi-family Residential

4.7.1.1 Key urban design role

These buildings are the face of the residential neighbourhoods. They present opportunities for highly-varied architectural responses through differences in form, scale, materiality and character, contributing to a rich pedestrian environment and scale.

4.7.1.2 Form and character

1. Ground-level apartments: Designed to blend into the whole building facade, the two storey units at grade will contribute to a richer, more human scale at the pedestrian level. With front doors 0.6m (2 ft) above the street level, these two-storey units generally follow the principles of a townhouse design. Raised entry areas provide comfortable semi-private space for a garden and patio.
2. Massing: A strategy of setting floors back at the upper levels will help to soften the building massing and increase access to daylight. Setbacks are especially encouraged in mid-block courtyards for daylighting and privacy as well as providing generous deck space for residents. On all buildings, where possible, interesting roof shapes are encouraged to enrich the overall texture and visual amenity of the development. These may include curving forms, deep overhangs, sloped planes and the like, appropriate to an understated modernist expression. Stair enclosures serving roof decks can also add to this diversity of form.
3. Green roofs: It is anticipated that some green roofs will be provided on most of the concrete buildings. This principle would further enhance the character of the building significantly. Green roofs should be designed as visual amenity as well as an attractive environment for outdoor common space. In wood framed buildings, roof vegetation should be incorporated in self contained planters.
4. Transparency and legibility of indoor public spaces: Common interior spaces such as entry lobbies, stairs, exercise rooms, lounges and the like should be afforded as much transparency to the exterior as possible, encouraging a visual connection between these spaces and the exterior community.



Figure 32: These images convey five different approaches to similar sized buildings that still relate to one another through their simple geometries and common materiality (Representational of character only).

4.7.2 Mixed Use Buildings

4.7.2.1 Key urban design role

On commercial streets, including Dewdney Trunk Road and 224 St, the multi-family form is combined with retail and service commercial uses at the ground floor. This provides a continuum of mixed-use frontages that attracts pedestrian activity and creates a vibrant public realm.

4.7.2.2 Form and character

1. Design expression: The potential for mixed-use buildings to enrich the Town Centre is high and should be incorporated wherever possible in the architectural design. In responding to the urban design roles mentioned above, the design should:
 - A. Provide a comfortable scale for a walkable shopping environment; commercial tenancies (i.e. storefronts) are to be divisible into increments of between 5.5 and 7.5 meters.
 - B. The emphasis should be towards boutique style commercial tenants and not the typical large anchor tenant (The frontage would be greater, providing more exposure). Creative storefront merchandising will also be necessary to provide views into these spaces, as well as views to the street from within the commercial units.
 - C. The residential portions of the mixed-use buildings will generally follow the design principles for multi-family buildings as previously described.
2. Massing:
 - A. Generally 4 to 6 storeys in height. The primary aim in the design of mixed-use buildings is to create a streetscape composed of a series of different but complementary building frontages ranging in width from 7.5 to 20m.
 - B. An overheight ground floor of approximately 3.6 m provides good flexibility for a variety of retail uses as well as streetfront space.
 - C. Setbacks at the uppermost residential storeys of mixed use buildings will be encouraged to create a comfortable street level scale and increased privacy for residents. Setbacks should be used to help differentiate between the commercial and residential parts of a building. Each use should be given a clear expression through a change of materiality, a change of plane or a combination of both.



Figure 33: These images demonstrate various approaches to delineating between the ground floor commercial and upper floor residential uses. (Representational of character only).

4.7.3 Townhouses and Ground Level Apartments (T&GLA's)

4.7.3.1 Key urban design role:

The T&GLA's play a valuable role in the neighbourhood character of ERA, contributing a smaller-scale form. Although stand-alone T&GLA's is not the prevalent typology in the development, most residential buildings include this type of unit concept at grade to extend this intimate, pedestrian-friendly character throughout the public realm.

4.7.3.2 Form and character:

1. **Massing:** T&GLA's forms and articulation should clearly distinguish individual units both in plan and elevation. This can be achieved in a variety of ways including sloped roof, recesses, vertical 'framing', etc. Consistent with the concept of a small-scale form, setting back the frontage above the second level is encouraged to further reinforce the legibility of each unit. Architectural elements such as individual roofs and entry canopies should be considered. Simple roof shapes can provide further architectural interest.
2. **Access to outdoors:** Semi-private spaces should be designed to further distinguish individual units while providing a rich landscaped buffer between units and the public realm. At upper levels, balconies, terraces and roof decks are strongly encouraged to provide a more private outdoor experience as well as providing increased articulation.
3. **Simple, clean expression:** T&GLA's designs should reflect the current modern trends around the world - using solid planes to contrast with generous areas of glazing and clearly defined outdoor spaces.



Figure 34: These images demonstrate the successful incorporation of T&GLA's in larger scale buildings (Representational of character only).

4.7.4 Tower

4.7.4.1 Key urban design role

The tower typology is paramount in the overall development fabric. Location and height are strategically utilized to:

1. Provide a three dimensional composition that 'frames' the mixed use core of the Central Neighbourhood, giving it legibility from Dewdney Trunk Road; and
2. Assist the pedestrian with neighbourhood orientation — for example, the tower at the intersection of Dewdney Trunk Road and 224 street will clearly mark the location of the proposed Public Plaza.

4.7.4.2 Form and character

1. General expression: Anticipating the coming changes to the BC Building Code in addressing thermal performance, ERA looks to introduce an aesthetic for towers that recalls the simple, strong and clean expression of elements associated with modern European design; with the goal of achieving balanced window to wall ratios in alignment with current thermal performance requirements, towers should strive to achieve a composition of simple forms and massing.
2. Massing: Tower floor plate sizes are based on an average gross floor area of 650 m² (7,000 sq ft).
3. Articulation and orientation: It is intended that towers be generally oriented with the long axis running north-south to optimize access to daylight and minimize the impact on upland views.
4. Consideration should be given to how the towers are terminated at the upper levels. Tops of towers should be articulated to ensure continuity of the design throughout the buildings.



Figure 35: The images above showcase a variety of tower designs that utilize punched openings and a balance between glazing and solid walls (Representational of character only).

4.7.5 Commercial/Offices

4.7.5.1 Key urban design role

The two commercial buildings in Phase 5 along Dewdney Trunk Road play lead roles as gateways marking entry to the new green space located to the north of the site. In response to their high profile locations, these buildings are intended to provide for uses that will help stimulate the vitality of the pedestrian realm and invigorate the neighbouring green space.

4.7.5.2 Form and character

1. Design expression:
 - B. In the context of the contemporary architecture proposed throughout ERA, the commercial buildings provide an opportunity for unique and innovative design. A dramatic expression responding to their prominent placement and their specific use is expected to set them apart and enhance the architectural diversity of the development. At the same time, these buildings should complement the scale, giving careful attention to articulated streetwalls that enhance the pedestrian experience.
 - C. Following the principles of good sustainable design, these buildings will also strive to deliver an enhanced workplace for occupants. Access to daylight, glazed stair enclosures to encourage their use, access to outdoor space, operable windows and envelope systems designed for optimal energy conservation are anticipated initiatives that will impact the exterior building design.
 - D. Addressing the above goals, designers are challenged to create a new model for commercial buildings in the ERA development.
2. Massing:

Significant ground floor height gives the commercial buildings a scale appropriate to their gateway roles and to their frontages on Dewdney Trunk Road. Designs should seek to create visually engaging frontages at this level, ensuring these significant masses are broken down into increments to create a more comfortable scale for pedestrians. Active spaces should be located at the perimeter, as opposed to closed individual offices. Public lobbies, located along Dewdney Trunk Road, Garden Street and 223rd Street should be highly transparent and clearly visible for pedestrians.
3. A livable workplace:
 - A. Occupant comfort is a primary aim for these buildings. As one of the simplest but most effective ways of creating livable study and office environments, good daylighting should be an integral part of the building design. This starts with work stations located within 9m of exterior glazing but can be optimized by bouncing light deeper into the space using light shelves and tempering glare with the use of exterior sunshades. Centre atrium can also increase recess to daylight.
 - B. Control of one's workspace environment is another aspect of user comfort and can influence the external expression. Options for occupants include operable windows, mechanical systems zoned for smaller areas where they can respond more quickly to individual demands and access to outdoor space.
 - C. Balconies, terraces and roof decks are all opportunities for a breath of fresh air, a coffee break or an ad hoc meeting. Their contribution to the exterior architecture can also be very positive, helping to articulate the mass in a variety of ways as well as bringing life to the street.



Figure 36: These images demonstrate street-friendly commercial buildings that enhance the vitality of the neighbourhood (Representational of character only).

4.8 General Residential Building Design Guidelines

4.8.1 Building materials

- A. Building elevations emphasizing one or two natural building materials, in addition to glazing, are strongly encouraged
- B. Accent materials providing colour, interest and contrast to the overall building design are encouraged, e.g. ceramic tile or glazed brick and coloured glass and spandrel panels are recommended for their durability and wide range of colour choices
- C. Well crafted, durable materials that support sustainability and Town Centre themes are expected throughout
- D. Natural building materials including wood, stone and brick should be included in the expression on lower floors and along the streetwall. Exposed concrete is acceptable to use on the commercial areas but in a smaller amount
- E. Functional screens, shading devices and other passive solar design elements that complement the architecture are recommended
- F. The use of red brick is discouraged
- G. The level of design quality required for streetscape elements should be maintained
- H. If heavy timber and engineered wood elements are used especially along the base of the building and at entrances they should be protected from weathering using best building practices and appropriate finishes that preserve the natural colour and texture
- I. Colours should be chosen to complement the palette of natural stone and wood: tones of grey are considered most appropriate
- J. Soffit materials should be consistent with the building's overall durability and quality
- K. Wood, fibre cement or metal soffits are preferred where feasible
- L. Clear vision glass is preferred over tinted products
- M. Material transitions should avoid a "wallpaper" look



Figure 37: Examples of different material expressions (Representational of character only).

4.8.2 Building colours

The use of colour contributes greatly to the sense of a building's cohesiveness within its neighbourhood. Generally, successful colour schemes are ones that use two or three harmonious colours: a predominant body colour, a second colour for large architectural elements (such as doors), and a third for decoration. The colour of the roof material is an important component of the building's overall colour scheme. The following points should be always considered:

- A. The primary colour palette within the Town Centre should be more subdued than highly expressive and trendy.
- B. The use of brighter, complementary accent colours to offset low winter light levels and increase visual interest.
- C. Colour combinations should strive to be subtle and as timeless as possible.



Figure 38: These images demonstrate the proper use of colour to achieve accents in the facade of various buildings and elements (Representational of character only).

4.9 Commercial Retail Units (CRU)

These guidelines ensure that the commercial retail units (CRU) located at the ground floor of the building create an active pedestrian realm, that is an engaging and well-populated environment with a variety of uses and activities.

4.9.1 CRU design principles.

The following principles should be implemented in all phases of the development where CRU's are included:

- A. Provide a minimum of 0.6m setback from the property line for the storefront facades.
- B. Locate active commercial uses on the ground floor adjacent to the sidewalk, including retail, restaurants, and personal service uses. Ground floor street frontage space is to be predominantly for active, pedestrian-oriented uses.
- C. Provide continuous storefront windows, open air store frontages, and frequent, highly visible entrances for ground floor commercial uses adjacent to the street and sidewalk.

4.9.2 Materials and proportions

- A. Glazing is encouraged in all retail units and should be predominant in all the building facades.
- B. High quality and durable trim materials, such as anodized or painted aluminum, are permitted and encouraged for windows and door trims.
- C. Continuous store windows and frequent, highly visible display storefronts along the street are a paramount feature as they provide visual interest and promote walkability.
- D. Open air store frontage type is acceptable along the internal corridors of the development. It brings the retail activity to the street, engaging pedestrians who walk by.
- E. Retail glazing is to be clean and not include frosted glass.

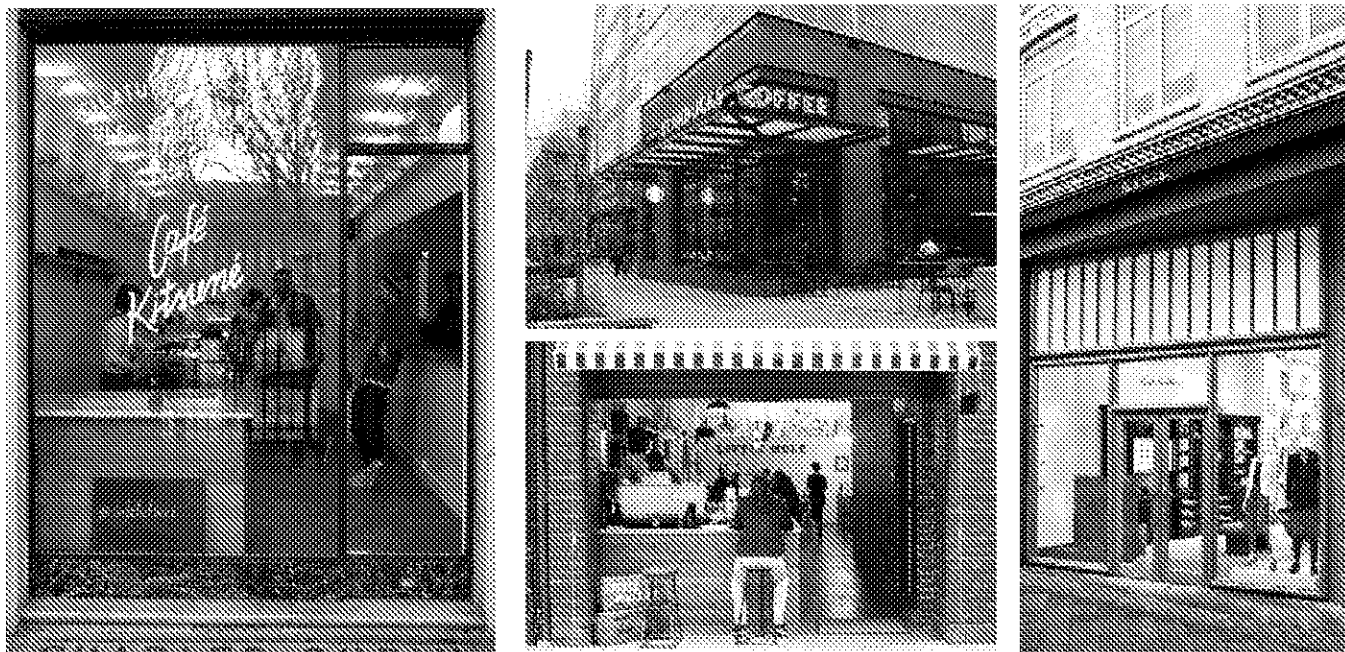


Figure 39: The images above show a variety of glazed entrances as examples of what can be implemented in the various types of CRU's spaces throughout the development (Representational of character only).

4.10 Entrances

The design of an entrance has a significant influence on both the appearance and functionality of the building. Entrances provide a focal point for residents and visitors and serve to welcome people into the building.

An entrance should be clearly visible and accessible at all times. It should promote and create a positive impression for all building users and make them feel welcome. If an entrance is hard to find or if it is difficult to access due to heavy doors or narrow door width, it creates a poor first impression and may make some people feel less welcome or even excluded.

The position of an entrance may be highlighted with architectural features such as a canopy or a door recess. Emphasis should be placed on recessing accesses to avoid encroachment into City's right-of-ways with awnings or canopies. A change in surface texture of the pavement or forecourt may help to signal the location of an entrance, particularly for people with visual difficulties.

4.10.1 Key Design Elements

All building entrances within the development would take into consideration the following:

- A. Locate all customer entries and entryways to be directly visible from the public sidewalk, and accessible from public and private walkways. Corner buildings are encouraged to have corner entries but it is not mandatory.
- B. Provide a pedestrian walkway from the public sidewalk to the entry frontage of buildings set back from the street edge.
- C. Design public street facing residential facades with individual entries, such as steps, porches, and paths from living units to the street to help break down the scale of multi-unit buildings.
- D. Audio clues, such as a small fountain or rustling plants, and olfactory features such as fragrant plants can also assist.
- E. Artificial lighting can highlight the entrance to a building and make it more obvious at night for everyone.

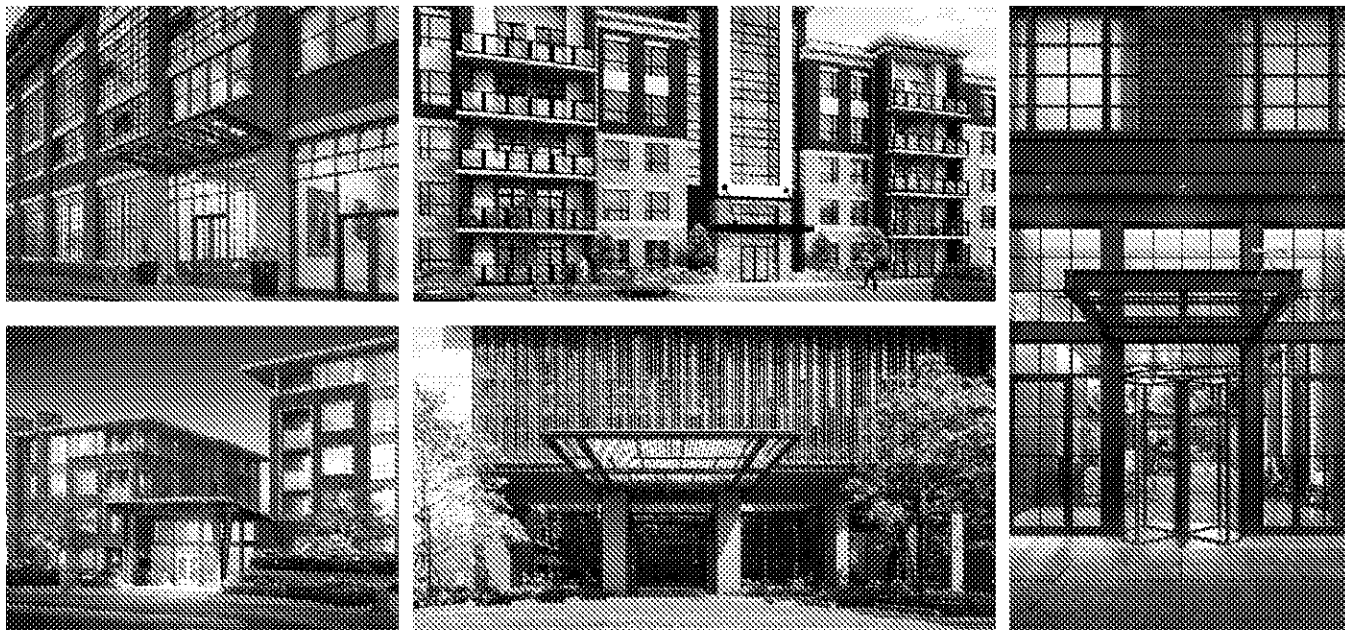


Figure 40: These images demonstrate the proper use of canopies to help mark the main entrance of the building (Representational of character only).

4.11 Awnings and Canopies

- A. Glazed canopies with laminated clear glass and/or diffused glass must be used above all CRU's along Dewdney Trunk Road.
- B. Canopies should extend from the face of the building to provide sufficient pedestrian weather protection.
- C. Any canopy that might encroach into the City's right-of-ways must be designed to be removable and are subject to City approval.

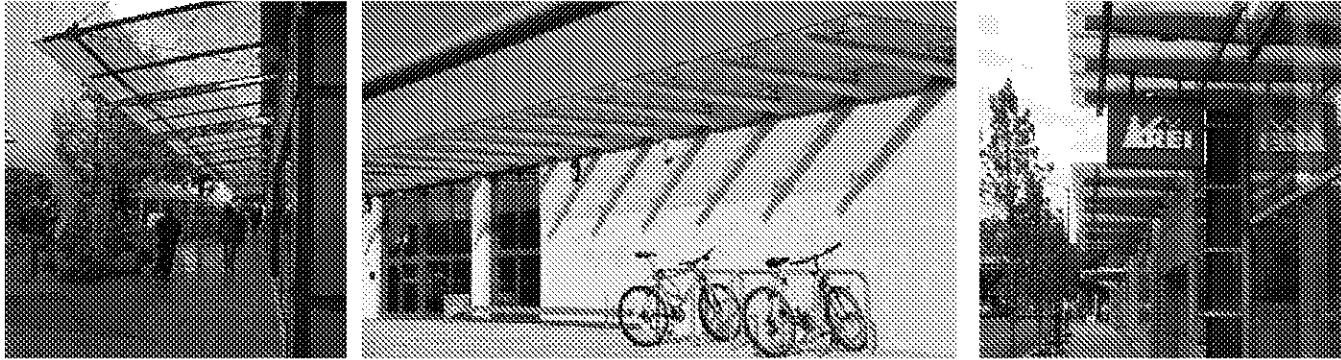


Figure 41: Canopies must provide shelter to pedestrians, visitors and retail customers without obstructing daylight to the CRU's (Representational of character only).

4.12 Signage and Lighting

- A. Entry points must be clearly identified through architecture, signage and landscape design.
- B. Signage to parking must be clearly visible.
- C. Tailored merchant signs and lighting will serve to personalize each storefront. Signage is encouraged to be hung from underside of glazed canopy.
- D. Simple surface mounted fixtures should be considered to provide the required light levels at entries. Large backlit signs are not encouraged.
- E. Provide decorative elements to add personality and individuality to different establishments.
- F. Use 'warm light' sources (LED preferred) for superior visual acuity and enhanced perception of light. No higher than 3100 K.

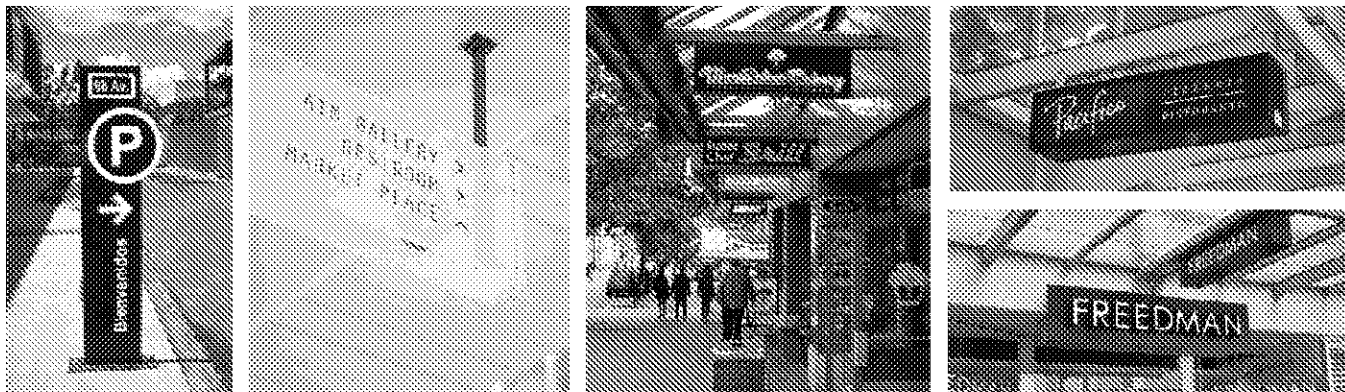


Figure 42: Signage within the development should be clear and easy to identify. (Representational of character only). Any other signage should meet City standards.

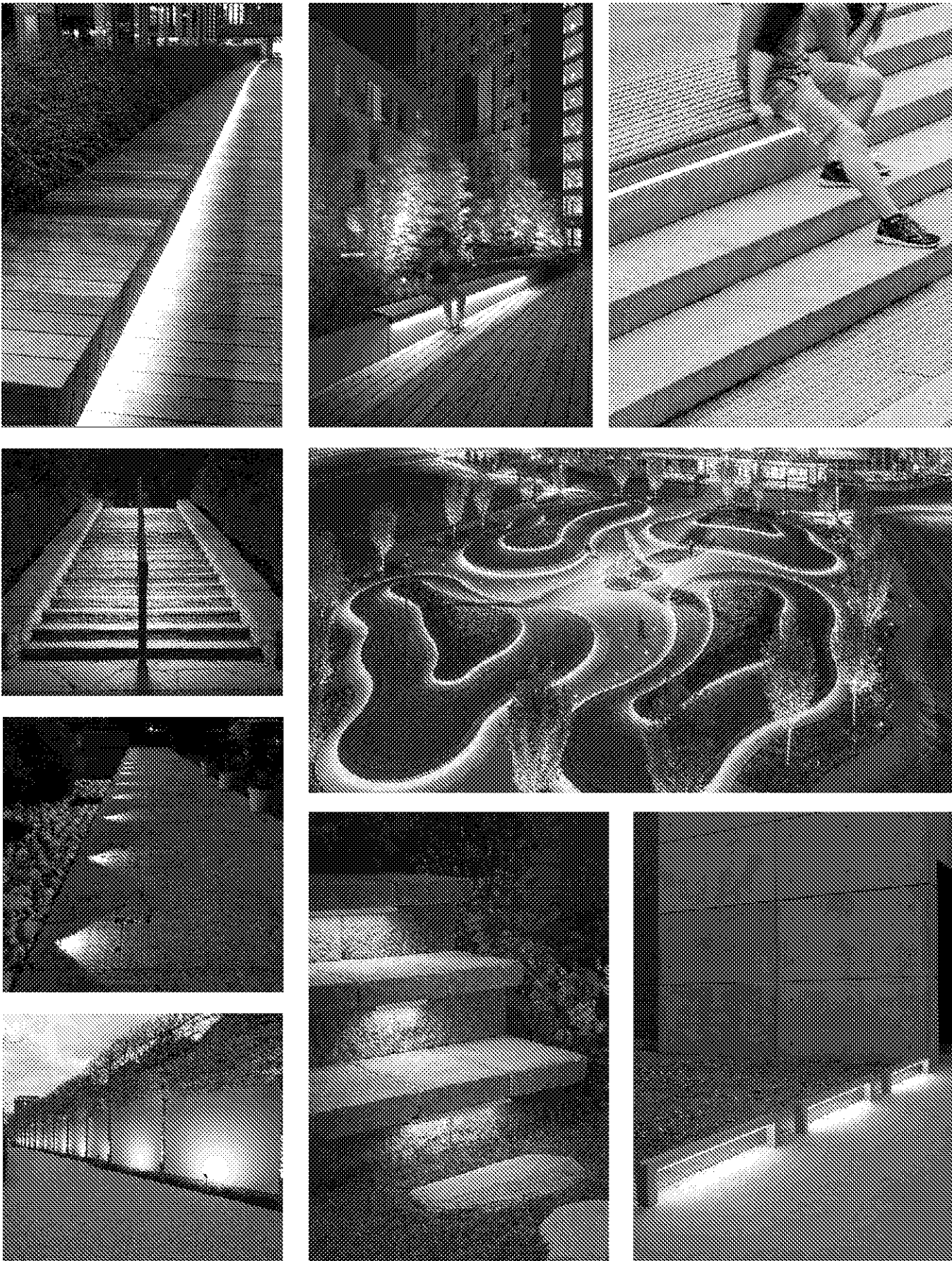


Figure 43: Examples of lighting fixtures used to identified entry points and to accentuate exterior paths (Representational of character only).

4.13 Digital Information Board

Located in Phase 7 of the development, the digital information board is meant to become a versatile digital tapestry capable of conveying multiple graphic mediums. It should also be conceived in such a way that acts as a public art element.

The form and structure has intentionally not been defined at this time but should compliment the eventual architectural expression of adjacent buildings and the public plaza along 224 Street.



Figure 44: Illustrative images of information boards examples (Representational of character only).

APPENDICES

ERA Design Guidelines Checklist for Development Permit Applications Phase 2 - 7

In accordance with the conditions set by City Council permitting the ERA development site to be pre-zoned, these design guidelines have been established for all phases of the ERA site to ensure the comprehensive developed is achieved on a phase by phase basis.

These guidelines govern the design requirements for each phase to be firstly assessed against the form and character guidelines approved by Council and summarised below; and secondly pursuant with Section 8.11 of the Official Community Plan. The checklist for these guidelines are to be appended to this checklist to demonstrate consistency is achieved.

This checklist is to be prepared by the architect of record for the project to demonstrate the proposed design was developed in accordance with the ERA guidelines. Please assess and describe the compliance of the proposed design of the project with respect to the ERA Key Guideline Concepts and with the ERA Form and Character Guidelines.

Description of the both the ERA and the OCP Key Guideline Concepts should be suitable and sufficiently descriptive to quote to ADP and to Council by for File Managers to quote in Development Permit Application Reports to Council. For the ERA and the OCP Form and Character Guidelines, clearly describe how the proposed design complies with each of the listed guidelines, or describes why a guideline is not complied with or why it is inapplicable.

Attached to this checklist are the following:

- General Town Centre Guidelines Checklist
- West Precinct Guidelines Checklist

This set of checklists are to accompany Development Permit Applications for each phase and submissions to the ADP.

| 1.3.2 Key design elements | Describe how this project and the design complies |
|--|---|
| A. Residential components of varying scale and density, positioned to maximize daylight penetration into the site and to create visual interest from every corridor. | |
| B. A prominent east-west public pedestrian open space and pedestrian way from Plaza Street to 224 Street, punctuated with activity areas (including child play, central activity plaza with theatre-like seating, and a major commercial plaza) and linkages to semi public and private space paths and mews and the City sidewalk system. | |
| C. Mews surrounded by both residential and commercial activity to ensure there are plenty of "eyes on the street" at all hours of the day. | |
| D. Public plaza in the east portion of the site, lined with commercial shops (inner "shopping row") and containing significant surface treatment, landscaping, pedestrian amenities, placemaking elements and a public art element. | |

| 1.3.2 Key design elements | Describe how this project and the design complies |
|---|--|
| E. An attractive and vibrant streetscape along Dewdney Trunk Road with building spaces designed to accommodate ground level shops and services, and access to upper level non-residential components where they occur. | |
| F. Strategically placed commercial spaces to create an intimately scaled shopping corridor combined with residential use above. | |
| G. Sustainable design solutions to maximize rooftop space for green houses, gardens and power generation. | |
| 2.3 Key Urban Design Principles | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable |
| A. Pedestrian friendliness Streetscapes and public walkways are to be articulated using appropriate scale, character and texture. Street trees, front doors (from ground-level apartments and retail storefronts), different scaled plantings and a variety of paving treatments are to be used to help define these paths and enhance the pedestrian experience. Security is also to be considered through the use of lighting and lower planting along path's edges. | |
| B. Public accessibility and permeability Provide clearly defined barrier-free access in both North-South and East-West directions through the site. Landscape elements, both soft and hard are to define and enhance these paths. | |
| C. Livability The overall layout and design of individual buildings and dwellings has been designed to create a variety of different living experiences and to provide a high standard of privacy, usable open space, and recreational amenities. This is achieved through different massing, height and configuration of buildings throughout the various phases of the overall development. | |
| D. Neighbourliness The form and layout of the site plan was driven by a desire for open space and connectivity. A variety of open and public spaces will facilitate community gathering. | |

| 2.3 Key Urban Design Principles | Describe how this project and the design complies |
|--|--|
| <p>E. Diversity Diverse housing types including a range of small and large units, condos and ground-level apartments, and low-rise and high-rise buildings will appeal to a large cross-section of the community. Appropriate architectural expression should be given to each as described in other sections of this document.</p> | |
| <p>F. Sustainability Sustainable design and development principles and practices will be employed such as, water efficient landscape design and rain and storm water management.</p> | |
| <p>G. Placemaking Public spaces acting as nodes making a positive and memorable contribution to Maple Ridge Town Centre. These spaces should be designed to facilitate a wide variety of civic activities and provide visual interest for the residential buildings surrounding them throughout the use of hardscape, landscape and plazas.</p> | |
| 2.5.2 Key Principles | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable |
| <p>A. The redevelopment of the ERA site will feature a mobility network that prioritizes pedestrian circulation and comfort above other transportation modes. Sidewalks and pedestrian street crossings will be continuous, universally accessible, barrier free and clearly designated; Special attention should be paid to crossings at roadways along the East-West Central Promenade; where the pedestrian should be prioritized and protected using an elevated crosswalk (aka. "speed table") with appropriate markings for vehicular traffic & tactile warnings accessible design.</p> | |
| <p>B. Accessibility Standards and Best Management Practices (BMP) should be applied generously wherever possible in all aspects of landscape design.</p> | |
| <p>C. In considering the soil conditions of the geotechnical report, the landscape design should maximize soil volumes on-site to increase stormwater detention, prior to infiltration to ground. Additional measures of detention & retention may be required on-site to support the development of each phase. Refer to Rain and Stormwater Best Management Practices Section 2.6.4; using appropriate devices to reduce the impacts of runoff over proposed impervious areas.</p> | |

| 2.5.2 Key Principles | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
|--|---|
| D. Landscape design should take into consideration solar exposure, and appropriately utilize and orient deciduous shade trees to create spaces that are sustainable and useable year-round, while maximizing sunlight wherever possible. | |
| E. Landscape design should take into consideration the human scale when determining a hierarchy of circulation paths, heights of walls and other elements that delineate programmatic spaces, screening and layering of planting elements, and arrangement of furnishings. | |
| F. Landscape Design elements should complement the architecture to maintain a cohesive legibility within various 'Character Zones'. | |
| G. A hierarchy of wayfinding elements (including signage) should be carefully considered and incorporated to allow ease of navigation particularly for those visiting the site, but also as elements that add to the identification and differentiation of various 'Character Zones'. The elements of effective wayfinding being (1) landmarks or other immediate identifiable elements to infer current location and help people to orient themselves, (2) clear routes to destinations, (3) a hierarchy of well defined paths, nodes, and edges of programmed space. | |
| H. Public art and/or landscape features -- particularly that integrate either water or lighting -- should be considered at prominent locations to reinforce the importance of the Central Promenade and navigability of the development. | |
| I. Lighting design should be an integral part of landscape design. While the architecture may provide ambient lighting, the landscape should aim to -- at a minimum -- provide path lighting along accessible routes. Area lighting may be used as the density increases and the nodes become larger and more commercial/public. Additionally planting may be accented with spotlighting or various other effects to animate spaces that will be used beyond dusk. | |
| J. Landscape design at the streetscapes should create a continuous and cohesive public realm. Private areas along streetscapes should be vertically separated wherever possible to provide privacy in the instance that the space is actively used, as well as eyes on the street. In these instances, the landscape should still contribute to the visual enhancement of the public realm through use of interesting materials, and ideally, planting material separation. | |

| 2.5.2 Key Principles | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
|--|---|
| K. Walls and fences should be integrated as appropriate where stronger demarcation of private space is to occur. The material used should reflect the Character of the architecture as appropriate to that zone. Consideration of Crime Prevention Through Environmental Design (CPTED) should be forefront when determining heights, with ideal wall heights being 0.61m, with wall-mounted step-lighting at 0.36-0.46m height and a maximum of every 5.0 m on centre unless otherwise noted. | |
| L. Appropriate soil volumes and drainage layers must be used to ensure the vibrancy of plant material into maturity. A minimum of 0.46m soil depth must be provided to all plants (noting this refers to most shrubs, perennials, and groundcovers without rhizomatous root structures; lawns & extensive green-roof sedums excepted with typical depths of 0.30m), with ideal depth being 0.61-0.76m. All trees require a minimum depth of 0.76m on-slab and an ideal depth of 0.91-1.2m where applicable off-slab; soil may be locally bermed-up to meet this condition. All trees require a minimum soil volume of 10 cbm.; this requirement may be relaxed with discretion in raised planted beds on-slab, where smaller trees may be planted to provide solar shading or ornamental trees planted for aesthetics (noting that these trees may not be counted by the City as "replacement trees" as their mature height & canopy will be limited). | |
| 2.8.1 Key design elements | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| A. Must be a major portion of the block, with at least two sides open to the street. | |
| B. Located and oriented to maximize sunlight access throughout the day and provide uses that take advantage of the sunny location (e.g. cafés and patios). | |
| C. The edges of plazas should be lined with active uses at-grade, including building entrances, to animate and support the open space. Spill-out spaces, such as patios should be encouraged. | |
| D. At least one edge is open to the public sidewalk. The remaining edges are comprised of building facades (either one building or multiple buildings), potentially with mid-block pedestrian connections. | |

| 2.9.1 Key design elements | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
|--|---|
| E. Must be located at the same grade level as the public sidewalk. Where changes in grade are an important element of the overall design and programming, clear and direct access from the public sidewalk must be accommodated, and ensure universal accessibility. | |
| F. Should provide continuous weather protection in the form of canopies or arcades at the perimeter of the space. | |
| G. Plazas may contain a primary building entrance. | |
| H. Small scale elements should be used to create a human scale, and to define smaller sub-areas within the plaza for ample seating and gathering in the sun and shade. | |
| I. Creative and dynamic solutions for ample areas of seating should be provided. | |
| J. Provide pedestrian scale lighting at appropriate locations. | |
| 2.9.1 Preliminary Neighbourhood Park Guidelines | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 1. Large topographical manipulations to create a theatre and amphitheatre that should include a water feature (and possibly public art lighting features) to draw in visitors and drown out nearby noise pollution. Activating this central space will help ensure people are spending time or walking through during the daytime and evening. | |
| 2. The edges of the square should allow room for restaurants and cafes at the Office/Future Commerical Units to spill out into the open space. | |
| 3. A significant portion of the plaza/park should maintain open paved area to offer the opportunity for a variety of programmed activities such as vendors, performers, exhibitions, outdoor recreation, etc. | |
| 2.10.1 Semi-Private Amenity Gardens | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 1. Residential patios at podium level will receive additional patio space with raised planters on slab, with hedging and dense planting to screen from semi-private amenity space. | |
| 2. Amenity space will include a variety of programming; e.g. open turf space for sport play, relaxing trellised decks for passive activity, children's playgrounds, agricultural plots, etc. | |
| 3. Green roofs on podiums and towers. | |

| | |
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| 2.10.2 Private Patios | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 1. Use layers of landscape moving from shorter plants to taller plants as a transition from the public realms to the private realms. | |
| 2. Use landscaping, such as hedging and taller plants, to buffer views into homes and provide privacy for yards and entries. | |
| 3. Use low fences, maximum height of 1.0m (3 ft), to define extents of private yards and provide secure areas for residents, particularly along the mews and greenways. | |
| 3.1.1 Cycle Lanes and Pedestrian Paths | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| A. Provide sidewalks within residential projects, connecting from the street or driveway to unit entrances. | |
| B. Locate buildings to be contiguous wherever possible, and make accommodations for pedestrian circulation between adjacent businesses and sites. | |
| C. Where new on-street parking is located, provide pedestrian walkways that connect directly to the development. | |
| D. Provide pedestrian walkways with landscape amenities from within public areas to shop entries, and to other pedestrian oriented uses and destinations. | |
| E. All public areas must be fully accessible. Ramps, stairs, sidewalks, site furniture, crosswalks, and paving materials must all be designed to allow everyone to enjoy the community and outdoor commercial amenities. | |
| 3.1.2 Internal Pedestrian Walkways | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 1. Major routes serving larger numbers of pedestrians should be a minimum of 3.5m wide, and should be constructed with high quality paving materials such as concrete and/or stone pavers. These paths should meet BC Building Code requirements for wheelchair accessibility, either as part of the main route or on an alternate alignment. | |
| 2. Secondary routes and those leading to private residences should be a minimum of 1.5m wide, and should be constructed with materials such as concrete and/or stone pavers. Providing safe and comfortable access by individuals with limited mobility. | |

| | |
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| 3.1.2 Internal Pedestrian Walkways | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 3. Paving at pedestrian paths should be designed to utilize concrete, stone and/or concrete pavers combined with textured finishes for ramps. The front edge of stair treads should be textured, and/or constructed with a material that visually signals the change in level. In cases where pedestrian and bike paths are adjacent, a separation may be created with roughly textured borders between the two zones. | |
| 4. Lighting may be incorporated into or adjacent to stairs and ramps wherever possible. | |
| 3.1.10.1 Key design principles | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| A. While unobstructed lines of sight must be maintained at intersections, crosswalks and entrances to individual buildings, screening elements for unsightly service areas and utilities should be provided. | |
| B. Tree placement should be determined in relation to other landscape features to define public, semi-private and private space, to direct and enhance views, and to soften street edges. | |
| C. Landscape material should use arrangements of spaces to create outdoor spaces that explicitly address the feeling of safety. | |
| D. Along internal roads, install street trees 8-10m on centre with minor variations to suit entry driveways and site utilities. Trees are to be a minimum of 7cm caliper. Trees along public streets are determined by City of Maple Ridge. | |
| 3.2.1 Dewdney Trunk Road | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 1. Public/off-site right-of-ways and sidewalks should conform to City's Design Criteria standards and coordinated design through Servicing Agreements. 2. Typical: A 1.2m wide exposed aggregate concrete boulevard strip will contain: A. Street trees in decorative metal tree grates ("Dobney Foundry, Ivy-48CAT" pattern, bare metal finish), in a continuous soil cell trench as required for sufficient volume (10 cbm/tree); | |

| | |
|--|---|
| 3.2.1 Dewdney Trunk Road | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>B. Site furnishings, including benches ("Victory Stanley RB-28", with centre arm rest, Black), garbage receptacle ("Victory Stanley SD-35", side door opening c/w lock provided by City, Black, and option for recyclable cage/basket) or as directed by the City.</p> <p>C. Street & pedestrian lighting standards.</p> <p>D. A 2.0m wide broom finished commuter sidewalk will be adjacent to the property line. Street parallel parking</p> <p>3. Additional to the off-site streetscape design, Plaza Street will have casual surveillance from residential patios that are vertically separated from the street for privacy.</p> | |
| 3.2.2 Brown Avenue | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>1. Street trees will be part of a continuous lawn boulevard</p> <p>2. On street parking</p> <p>3. A multi-use path for cyclists and pedestrians</p> <p>4. Boulevards either side of Avenue</p> | |
| 3.2.3 Mews / Pedestrian Way | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>1. 2.4m (8') wide pedestrian path</p> <p>2. Grass shoulders expand path surface to 4m wide to accommodate fire truck access</p> <p>3. Residential patios either side of path are raised to provide 'eyes on the street'</p> <p>4. Live work units (more urban in nature) adjacent Dewdney Trunk Road</p> | |
| 3.2.4 223 Street / Garden Street | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>Collector thoroughfares for pedestrians.</p> <p>Pedestrian-oriented with measures to reduce vehicular speed.</p> | |

| 3.2.5 224 Street and New Public Corner Plaza | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
|--|---|
| <p>1. 224 Street is realigned thus creating an open space on the east side</p> <p>2. Built form is pulled back from the street to create an open space that spans the street</p> <p>3. Paving treatment may extend across the street (contingent on additional study and coordination with the City) and could be closed for festivals. Plaza and pedestrian path to the east could accommodate a farmer's market and/or potential location of public art.</p> | |
| 3.2.6 East-West Promenade | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>At Residential</p> <p>1. 2.4m (8') wide</p> <p>2. Heavily planted</p> <p>3. Finer-grained in residential scale & character</p> <p>4. Places to sit and gather</p> <p>5. Incorporation of rain water infiltration</p> | |
| <p>At Neighbourhood</p> <p>1. Path widens at public park/plaza</p> <p>2. Incorporation of extensive tree canopy</p> <p>3. Plaza space to south (adj. Commercial/Office) is multi-use park space to north is a neighbourhood & community destination</p> <p>4. Park has berm to create interest & play opportunity on a flat site</p> <p>5. Incorporation of rain-water trench & water feature in plaza</p> | |
| <p>At 224 Street</p> <p>1. Path widens to 3m (10')</p> <p>2. Single row of large trees rain-water trench takes on a more urban character more hardscape & urban in nature adjacent to commercial</p> | |

| 4.1 Massing and Building Form | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| A. Throughout the development, medium-sized buildings are typically stepped back above the first 3-5 storeys of the building creating a lower podium that reduces the overall mass of the buildings at street level. This design solution defines the difference of uses within the building, such as residential units over retail and office spaces. | |
| B. Street walls (building faces directly adjacent to a street) should be between three and six storeys in height. If less than three storeys they are usually not well defined, and over six storeys they can be oppressive to most people. In most instances, street walls should step back above the third to fifth floor. | |
| C. The point towers should strive to achieve a height to width ratio of no less than 2:1 to 3:1 for the 21-storey building. When not possible, the massing of the building should encourage visual verticality through key architectural moves. | |
| D. The podium should be at least two but not more than five storeys high. This morphology permits small-scale retail storefronts on the commercial streets. This duality of commercial and residential creates a strong street presence, with the tower providing residential density that enlivens the neighbourhood and makes the local commercial activity economically viable. | |
| E. This form of building can also accommodate two storeys of ground-level apartments above the podium level without creating an overwhelming street front as long as the upper units are setback from the cornice line. | |
| F. A clear difference between the entrances to the towers and the residential units should be maintained throughout the project, keeping the street fronts for retail and residential units. | |
| 4.3 Setbacks | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| A. The typical residential setback is between 3 to 6 meters (10-12 ft), which is sufficient to provide an outdoor patio or stoop that can accommodate outdoor living and/or seating. | |
| B. Other commercial or retail areas have a minimum 0.6m (24 inch) setback along Dewdney Trunk Road which will increase the overall width of the sidewalk zone, while allowing for outdoor seating, signage or other requirements, while not impacting the path of travel on public sidewalks. | |

| 4.3 Setbacks | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| C. A setback between each phase of approximately 7.5 to 18.3m (25-60 ft) shall be maintained, to accommodate sufficient breathing room between buildings and with significant landscaping or other landscape features. | |
| D. Lastly, an approximate 19.2m (63 ft) setback is provided on the east side of Phase 7 to ensure sufficient area to create a civic gathering space that will extend across the street, integrating the development into the rest of its context. | |
| 4.4 Garbage / Recycling Staging Areas | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>4.4.1 Internal storage</p> <p>The solid waste and recycling storage in a building must be accessible to all occupants of the development, and be clean, well lit, and conveniently located for users to get access to, including those with restricted mobility. The location of such a room or rooms is preferred to be within the underground portion of a building, such as an underground parking level, permitting convenient staging on refuse collect day.</p> | |
| <p>4.4.2 Staging Areas</p> <p>Especially for multi-unit residential buildings, if containers are stored underground, they will likely need to be relocated above ground for servicing on collection day (by building staff or a container pullout service provider). Thus, a temporary storage area should be incorporated in the ground level portion of the building and site plans provided for collection vehicles to manoeuvre and to collect materials without impeding street, parking entrances and sidewalk circulation (ensuring no height clearance issues).</p> | |
| <p>4.4.3 Other Design Considerations:</p> <p>A. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces.</p> <p>B. Both internal and staging areas are to comply fully with the City Waste Collection policies.</p> <p>C. Any portion of the staging area at grade must be within private property sufficiently setback from streets and pedestrian areas.</p> <p>D. Loading areas, garbage containers and recycling containers shall be screened from any adjacent residential lot or from overlook from upper storeys, to a height of at least 2.5 metres [8 ft.] by buildings, a landscaping screen, a solid decorative fence, or a combination thereof.</p> | |

| 4.5 Building Sustainability Strategies | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| <p>1. Increased envelope thermal performance (both walls and fenestration),</p> <p>2. Increased energy efficiency of mechanical systems such as the inclusion of HRV's,</p> <p>3. inclusion of more passive strategies such as window shading on the applicable orientations,</p> <p>4. Green roofs</p> <p>5. use of low or classified Zero-VOC construction materials and finishes,</p> <p>6. Re-Use, Recycle or Use Rapidly Renewable materials</p> <p>7. Choose Third-Party Certified Products, etc.</p> | |
| 4.6 Parking Considerations For All Phases | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 4.6.1 All vehicle parking is to be provided in a below grade or above ground parking structure. | |
| 4.6.2 For above ground parking structures, the design of these parking structures should blend in with architectural elements of the main commercial or residential building, including the use of architectural or decorative grille or other similar elements for wall openings. Steel mesh is not recommended. | |
| 4.6.3 Parking provided is to be separated between commercial and office use parking spaces (i.e. nonresidential parking) and residential parking spaces (i.e. parking for residents and parking for their visitors). | |
| 4.6.4 There will be a security gate between the non-residential and residential parking. The security system will be designed to allow access to visitor's parking spaces by visitors, but retaining security to the portion of the parking spaces for residents should be recommended where the overall building configuration makes it possible. | |
| 4.6.5 Residential parking shall separate parking for visitors and parking for residents. | |
| 4.6.6 A gate into the parking area from the street level is optional as determined by circulation and security considerations. | |

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| 4.6 Parking Considerations For All Phases | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| 4.6.7 Pedestrian access, including those with restricted mobility, will be designed to provide safe and convenient access between the separate parking areas and the portion of the building the parking is intended to serve. Access from residential parking areas will take into consideration families with children and individuals using mobility assistance equipment. | |
| 4.7.1.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>1. Ground-level apartments: Designed to blend into the whole building facade, the two storey units at grade will contribute to a richer, more human scale at the pedestrian level. With front doors 0.6m (2 ft) above the street level, these two-storey units generally follow the principles of a townhouse design. Raised entry areas provide comfortable semi-private space for a garden and patio.</p> <p>2. Massing: A strategy of setting floors back at the upper levels will help to soften the building massing and increase access to daylight. Setbacks are especially encouraged in mid-block courtyards for daylighting and privacy as well as providing generous deck space for residents. On all buildings, where possible, interesting roof shapes are encouraged to enrich the overall texture and visual amenity of the development. These may include curving forms, deep overhangs, sloped planes and the like, appropriate to an understated modernist expression. Stair enclosures serving roof decks can also add to this diversity of form.</p> <p>3. Green roofs: It is anticipated that some green roofs will be provided on most of the concrete buildings. This principle would further enhance the character of the building significantly. Green roofs should be designed as visual amenity as well as an attractive environment for outdoor common space. In wood framed buildings, roof vegetation should be incorporated in self contained planters.</p> <p>4. Transparency and legibility of indoor public spaces: Common interior spaces such as entry lobbies, stairs, exercise rooms, lounges and the like should be afforded as much transparency to the exterior as possible, encouraging a visual connection between these spaces and the exterior community.</p> | |

| 4.7.2.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| <p>1. Design expression: The potential for mixed-use buildings to enrich the Town Centre is high and should be incorporated wherever possible in the architectural design. In responding to the urban design roles mentioned above, the design should:</p> <p>A. Provide a comfortable scale for a walkable shopping environment; commercial tenancies (i.e. storefronts) are to be divisible into increments of between 5.5 and 7.5 meters.</p> <p>B. The emphasis should be towards boutique style commercial tenants and not the typical large anchor tenant (The frontage would be greater, providing more exposure). Creative storefront merchandising will also be necessary to provide views into these spaces, as well as views to the street from within the commercial units.</p> <p>C. The residential portions of the mixed-use buildings will generally follow the design principles for multi-family buildings as previously described.</p> <p>2. Massing:</p> <p>A. Generally 4 to 6 storeys in height. The primary aim in the design of mixed-use buildings is to create a streetscape composed of a series of different but complementary building frontages ranging in width from 7.5 to 20m.</p> <p>B. An overheight ground floor of approximately 3.6 m provides good flexibility for a variety of retail uses as well as streetfront space.</p> <p>C. Setbacks at the uppermost residential storeys of mixed use buildings will be encouraged to create a comfortable street level scale and increased privacy for residents. Setbacks should be used to help differentiate between the commercial and residential parts of a building. Each use should be given a clear expression through a change of materiality, a change of plane or a combination of both.</p> | |

| 4.7.3.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| <p>1. Massing: T&GLA's forms and articulation should clearly distinguish individual units both in plan and elevation. This can be achieved in a variety of ways including sloped roof, recesses, vertical 'framing', etc. Consistent with the concept of a small-scale form, setting back the frontage above the second level is encouraged to further reinforce the legibility of each unit. Architectural elements such as individual roofs and entry canopies should be considered. Simple roof shapes can provide further architectural interest.</p> <p>2. Access to outdoors: Semi-private spaces should be designed to further distinguish individual units while providing a rich landscaped buffer between units and the public realm. At upper levels, balconies, terraces and roof decks are strongly encouraged to provide a more private outdoor experience as well as providing increased articulation.</p> <p>3. Simple, clean expression: T&GLA's designs should reflect the current modern trends around the world - using solid planes to contrast with generous areas of glazing and clearly defined outdoor spaces.</p> | |
| 4.7.4.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>1. General expression: Anticipating the coming changes to the BC Building Code in addressing thermal performance, ERA looks to introduce an aesthetic for towers that recalls the simple, strong and clean expression of elements associated with modern European design; with the goal of achieving balanced window to wall ratios in alignment with current thermal performance requirements, towers should strive to achieve a composition of simple forms and massing.</p> <p>2. Massing: Tower floor plate sizes are based on an average gross floor area of 650 m² (7,000 sq ft).</p> <p>3. Articulation and orientation: It is intended that towers be generally oriented with the long axis running north-south to optimize access to daylight and minimize the impact on upland views.</p> <p>4. Consideration should be given to how the towers are terminated at the upper levels. Tops of towers should be articulated to ensure continuity of the design throughout the buildings.</p> | |

| 4.7.5.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| <p>1. Design expression:</p> <p>A. In the context of the contemporary architecture proposed throughout ERA, the commercial buildings provide an opportunity for unique and innovative design. A dramatic expression responding to their prominent placement and their specific use is expected to set them apart and enhance the architectural diversity of the development. At the same time, these buildings should complement the scale, giving careful attention to articulated streetwalls that enhance the pedestrian experience.</p> <p>B. Following the principles of good sustainable design, these buildings will also strive to deliver an enhanced workplace for occupants. Access to daylight, glazed stair enclosures to encourage their use, access to outdoor space, operable windows and envelope systems designed for optimal energy conservation are anticipated initiatives that will impact the exterior building design.</p> <p>C. Addressing the above goals, designers are challenged to create a new model for commercial buildings in the ERA development.</p> <p>2. Massing:</p> <p>Significant ground floor height gives the commercial buildings a scale appropriate to their gateway roles and to their frontages on Dewdney Trunk Road. Designs should seek to create visually engaging frontages at this level, ensuring these significant masses are broken down into increments to create a more comfortable scale for pedestrians. Active spaces should be located at the perimeter, as opposed to closed individual offices. Public lobbies, located along Dewdney Trunk Road, Garden Street and 223rd Street should be highly transparent and clearly visible for pedestrians.</p> | |
| <p>3. A livable workplace:</p> <p>A. Occupant comfort is a primary aim for these buildings. As one of the simplest but most effective ways of creating livable study and office environments, good daylighting should be an integral part of the building design. This starts with work stations located within 9m of exterior glazing but can be optimized by bouncing light deeper into the space using light shelves and tempering glare with the use of exterior sunshades. Centre atrium can also increase recess to daylight.</p> | |

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| 4.7.5.2 Form and character | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>B. Control of one's workspace environment is another aspect of user comfort and can influence the external expression. Options for occupants include operable windows, mechanical systems zoned for smaller areas where they can respond more quickly to individual demands and access to outdoor space.</p> <p>C. Balconies, terraces and roof decks are all opportunities for a breath of fresh air, a coffee break or an ad hoc meeting. Their contribution to the exterior architecture can also be very positive, helping to articulate the mass in a variety of ways as well as bringing life to the street.</p> | |
| 4.9 Commercial Retail Units (CRU) | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>4.9.1 CRU design principles. The following principles should be implemented in all phases of the development where CRU's are included:</p> <p>A. Provide a minimum of 0.6m setback from the property line for the storefront facades.</p> <p>B. Locate active commercial uses on the ground floor adjacent to the sidewalk, including retail, restaurants, and personal service uses. Ground floor street frontage space is to be predominantly for active, pedestrian-oriented uses.</p> <p>C. Provide continuous storefront windows, open air store frontages, and frequent, highly visible entrances for ground floor commercial uses adjacent to the street and sidewalk.</p> | |
| <p>4.9.2 Materials and proportions</p> <p>A. Glazing is encouraged in all retail units and should be predominant in all the building facades.</p> <p>B. High quality and durable trim materials, such as anodized or painted aluminum, are permitted and encouraged for windows and door trims.</p> <p>C. Continuous store windows and frequent, highly visible display storefronts along the street are a paramount feature as they provide visual interest and promote walkability.</p> <p>D. Open air store frontage type is acceptable along the internal corridors of the development. It brings the retail activity to the street, engaging pedestrians who walk by.</p> | |

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| 4.9 Commercial Retail Units (CRU) | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| E. Retail glazing is to be clean and not include frosted glass. | |
| 4.10.1 Key Design Elements | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>All building entrances within the development would take into consideration the following:</p> <p>A. Locate all customer entries and entryways to be directly visible from the public sidewalk, and accessible from public and private walkways. Corner buildings are encouraged to have corner entries but it is not mandatory.</p> <p>B. Provide a pedestrian walkway from the public sidewalk to the entry frontage of buildings set back from the street edge.</p> <p>C. Design public street facing residential facades with individual entries, such as steps, porches, and paths from living units to the street to help break down the scale of multi-unit buildings.</p> <p>D. Audio clues, such as a small fountain or rustling plants, and olfactory features such as fragrant plants can also assist.</p> <p>E. Artificial lighting can highlight the entrance to a building and make it more obvious at night for everyone.</p> | |
| 4.11 Awnings and Canopies | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>A. Glazed canopies with laminated clear glass and/or diffused glass must be used above all CRU's along Dewdney Trunk Road.</p> <p>B. Canopies should extend from the face of the building to provide sufficient pedestrian weather protection.</p> <p>C. Any canopy that might encroach into the City's right-of-ways must be designed to be removable and are subject to City approval.</p> | |

| 4.12 Signage and Lighting | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
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| <p>A. Entry points must be clearly identified through architecture, signage and landscape design.</p> <p>B. Signage to parking must be clearly visible.</p> <p>C. Tailored merchant signs and lighting will serve to personalize each storefront. Signage is encouraged to be hung from underside of glazed canopy.</p> <p>D. Simple surface mounted fixtures should be considered to provide the required light levels at entries. Large backlit signs are not encouraged.</p> <p>E. Provide decorative elements to add personality and individuality to different establishments.</p> <p>F. Use 'warm light' sources (LED preferred) for superior visual acuity and enhanced perception of light. No higher than 3100 K.</p> | |
| 4.13 Digital Information Board | Describe how the proposed design complies with each of the listed guidelines, or describe why a guideline is not complied with or why it is inapplicable. |
| <p>Located in Phase 7 of the development, the digital information board is meant to become a versatile digital tapestry capable of conveying multiple graphic mediums. It should also be conceived in such a way that acts as a public art element. The form and structure has intentionally not been defined at this time but should compliment the eventual architectural expression of adjacent buildings and the public plaza along 224 Street.</p> | |

Project information

To be completed by the Architect on record for this project:

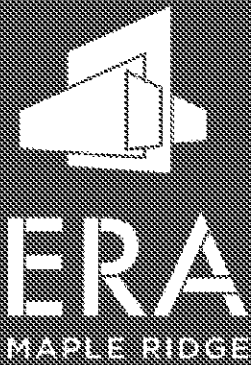
File number: _____

Date prepared: _____

Architect: _____

Print Name

Signature



PRIORITY AGREEMENT

BANK OF MONTREAL (the “Chargeholder”) is the holder of a Mortgage and an Assignment of Rents encumbering the Lands which the Mortgage and Assignment of Rents are registered in the New Westminster Land Title Office under number(s) CA8399561 and CA8399562 respectively (collectively, the “Bank Charge(s)").

The Chargeholder, being the holder of the Bank Charge(s), by signing the Form C General Instrument attached hereto as Part I, in consideration of the payment of Ten Dollars (\$10.00) and other good and valuable consideration (the receipt and sufficiency of which is hereby acknowledged and agreed to by the Chargeholder) hereby consents to the granting of this Section 219 Covenant and hereby covenants that this Section 219 Covenant shall bind the Bank Charge(s) in the Lands and shall rank in priority upon the Lands over the Bank Charge(s) as if the Section 219 Covenant had been registered prior to the Bank Charge(s) and prior to the advance of any monies pursuant to the Bank Charge(s). The grant of priority is irrevocable, unqualified and without reservation or limitation.

PRIORITY AGREEMENT

BRITISH COLUMBIA HOUSING MANAGEMENT COMMISSION (the “Chargeholder”) is the holder of a Mortgage and an Assignment of Rents encumbering the Lands which the Mortgage and Assignment of Rents are registered in the New Westminster Land Title Office under number(s) CA8510513 and CA8510514 respectively (collectively, the “Bank Charge(s)").

The Chargeholder, being the holder of the Bank Charge(s), by signing the Form C General Instrument attached hereto as Part I, in consideration of the payment of Ten Dollars (\$10.00) and other good and valuable consideration (the receipt and sufficiency of which is hereby acknowledged and agreed to by the Chargeholder) hereby consents to the granting of this Section 219 Covenant and hereby covenants that this Section 219 Covenant shall bind the Bank Charge(s) in the Lands and shall rank in priority upon the Lands over the Bank Charge(s) as if the Section 219 Covenant had been registered prior to the Bank Charge(s) and prior to the advance of any monies pursuant to the Bank Charge(s). The grant of priority is irrevocable, unqualified and without reservation or limitation.